



City of Huntington Beach Planning and Building Department
STUDY SESSION REPORT

TO: Planning Commission
FROM: Scott Hess, AICP, Director of Planning and Building
BY: Jane James, Senior Planner *JJ*
DATE: January 26, 2010

SUBJECT: **GENERAL PLAN AMENDMENT NO. 09-002, ZONING MAP AMENDMENT NO. 09-002, ZONING TEXT AMENDMENT NO. 09-007, CONDITIONAL USE PERMIT NO. 09-024, NEGATIVE DECLARATION NO. 09-006 (WARD GARFIELD SPECIFIC PLAN/RECREATIONAL VEHICLE STORAGE)**

APPLICANT: Michael Adams, Michael Adams Associates, 21190 Beach Boulevard, Huntington Beach, CA 92648

PROPERTY

OWNER: Frank Salomone, Southern California Edison, 14799 Chestnut Street, Westminster, CA 92683

LOCATION: 19118 Ward Street, 92646 (43.60-Acre Site Southeast of the Intersection of Ward Street and Garfield Avenue)

PROJECT REQUEST AND SPECIAL CONSIDERATIONS:

The applicant, Michael Adams Associates, is requesting to amend the land use and zoning designations on an existing approximately 43.60-acre site to remove the Residential Low Density zoning designation, establish a new specific plan, and allow four particular land uses: 1) electrical substation and associated transmission lines and towers, 2) wholesale/retail landscape nursery, 3) recreational vehicle storage, and 4) wireless communication facilities. The project consists of the following entitlement requests:

- General Plan Amendment: To amend the Land Use designation and add a Specific Plan suffix ("sp") to the existing Public land use designation;
- Zoning Map Amendment: To amend the existing zoning map from the current Residential Low Density designation to the Ward Garfield Specific Plan No. 16 designation;
- Zoning Text Amendment: To establish the Ward Garfield Specific Plan No. 16 document;
- Conditional Use Permit: To permit recreational vehicle storage on 13.52 acres of the total 43.60 acre specific plan area;
- Negative Declaration: To analyze the potential environmental impacts associated with the project and legislative amendments.

The Specific Plan development concept recognizes existing on-site uses including the Southern California Edison Ellis Substation and associated transmission lines and towers, Village Nurseries, and wireless communication facilities on existing transmission towers. The Specific Plan concept also allows

establishment of a new proposed 557-space recreational vehicle storage facility within the SCE right-of-way paralleling the Santa Ana River Channel.

CURRENT LAND USE, HISTORY OF SITE, ZONING AND GENERAL PLAN DESIGNATIONS

The project site consists of approximately 43.60 acres at the southeast corner of Ward Street and Garfield Avenue at the east end of the city. The site is owned and operated by Southern California Edison (SCE) who currently leases portions of the property to landscape nursery uses. A subdivision will be required to establish lease lines for the current and proposed uses. The subject property is located on one parcel as well as a portion of a second parcel.

The approximately 13-acre Ellis Substation consists of transmission equipment and power distribution lines along with the substation office building. The onsite operations are power distribution services with approximately eight to 10 employees. The facility is manned between 7:00AM and 5:00PM daily. The office building is approximately 2,000 square feet in size. A separate service truck parking building is also located on site. Access to the SCE substation is provided from Ward Street through the area leased to the nursery operation. No changes to the substation are proposed.

Village Nurseries Landscape Center has been in operation at the site since the 1970s (previously known as Nina’s Nursery). The nursery has operated through 5-year renewable lease agreements with SCE and consists of wholesale of in-ground and potted plants and trees. The business operates out of an open wood structure adjacent to two modular buildings joined to serve as an office. There are 10 employees and laborers employed at the nursery.

The wholesale nursery is open daily to the public with hours of operation generally between 7:00AM and 5:00PM. Customer access is provided from Garfield Avenue with business related access from Ward Street. On-site parking is currently provided along Garfield Avenue for approximately 20 cars. In addition, equipment stored at the site consists of cart movers, small trucks, and delivery equipment. Village Nurseries will consolidate their operation into the area surrounding the SCE substation in order to accommodate the new recreational vehicle storage operation. No additional facilities are proposed for the nursery operation.

LOCATION	GENERAL PLAN	ZONING	LAND USE
Subject Property:	P (Public)	RL (Residential Low Density)	SCE Substation, transmission towers and lines; Village Nurseries; Wireless facilities
North of Subject Property (across Garfield) in the City of Fountain Valley:	Fountain Valley – Specific Plan	Fountain Valley – Specific Plan	Orange County Sanitation District
East of Subject Property:	Undesignated	Undesignated	Santa Ana River Channel
South and West (across Ward Street) of Subject Property:	RM-15 (Residential Medium Density) and RL	RMP (Residential Mobile Home Park) and RL	Mariner’s Point Mobilehome Park, Arevalos Park, and single family resid.

APPLICATION PROCESS AND TIMELINES

DATE OF COMPLETE APPLICATION:

- Draft Negative Declaration: December 1, 2009
- General Plan Amendment, Zoning Text Amendment, Zoning Map Amendment: November 13, 2009
- Conditional Use Permit: November 13, 2009

MANDATORY PROCESSING DATE(S):

Within 180 days of complete application:
June 1, 2010

Not Applicable

Within 60 days from Negative Declaration Approval

General Plan Amendment No. 09-002, Zoning Map Amendment No. 09-002, Zoning Text Amendment No. 09-007, Conditional Use Permit No. 09-024, and Environmental Assessment No. 09-006 were filed on August 14, 2009 and deemed complete on November 13, 2009 (GPA, ZTA, and ZMA) and December 1, 2009 (EA). The application is tentatively scheduled for the Planning Commission meeting of February 9, 2010.

CEQA ANALYSIS/REVIEW

Draft Negative Declaration No. 09-006 analyzes the potential environmental impacts of the proposed project and entitlement requests. The draft ND concludes that the project will not have a significant environmental impact and no mitigation measures are required.

Draft ND No. 09-006 was prepared by staff and considered by the EAC on December 14, 2009. The EAC approved the processing of a negative declaration.

A 30-day public comment period on draft ND No. 09-006 commenced on December 17, 2009 and concluded on January 22, 2010. An extra week was added to the comment period to account for the holiday season. To date, the Planning and Building Department has received one comment letter from the Department of Toxic Substances Control. All comments received on Draft ND No. 09-006, as well as responses to each comment and any text changes to the draft ND, will be attached to the public hearing staff report for the Planning Commission's consideration.

COMMENTS FROM CITY DEPARTMENTS AND OTHER PUBLIC AGENCIES

The Departments of Fire, Public Works, Community Services, and Planning have reviewed the application and identified applicable code requirements (Attachment No. 4).

PUBLIC MEETINGS, COMMENTS AND CONCERNS

On September 22, 2009, the applicant held a community meeting to introduce the project to all property owners and tenants within a 1,000 foot radius of the site. Approximately four members of the surrounding community attended the meeting and discussed compatibility between the proposed recreational vehicle storage and the adjacent mobile home park.

PLANNING ISSUES

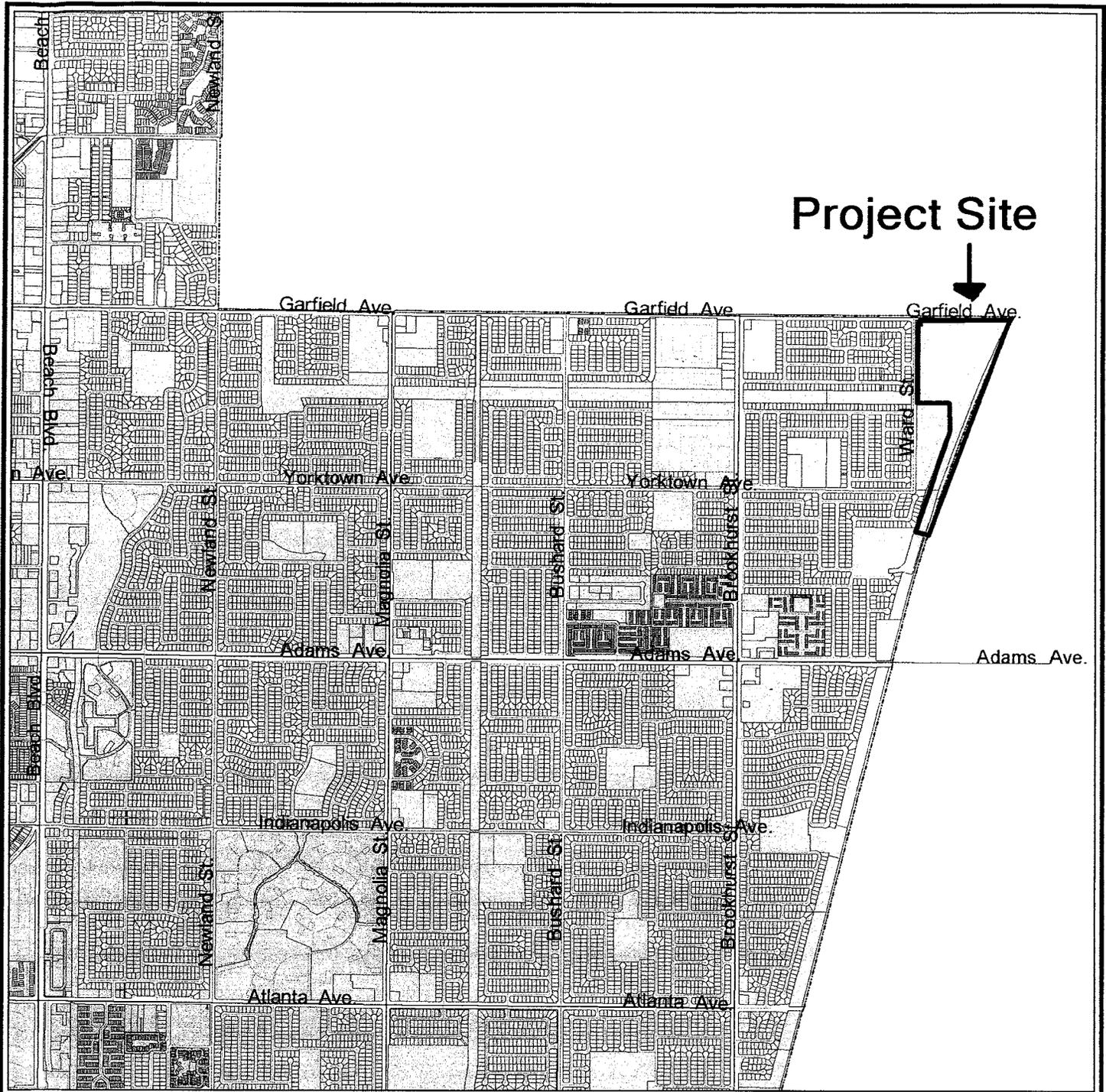
The Draft Ward Garfield Specific Plan No. 16 was distributed to the Planning Commission for a 30-day review period on December 22, 2009.

The primary issues for the Planning Commission to consider for the proposed project are:

- The proposed amendment to add the “specific plan” suffix to the Public General Plan land use designations;
- The proposed zone change from Residential Low Density to Specific Plan;
- The new land use concepts, development standards, and overall language of the proposed Specific Plan;
- Land use compatibility of the proposed 557-space recreational vehicle storage with the surrounding properties;
- Compliance of the proposed 557-space recreational vehicle storage with the provisions of the HBZSO and the proposed Specific Plan.

ATTACHMENTS

1. Vicinity Map and Site Aerial
2. Current and Proposed General Plan Designations (with surrounding designations)
3. Current and Proposed Zoning Designations (with surrounding designations)
4. Project Narrative dated and received August 14, 2009
5. Project Plans dated and received January 14, 2010
6. Code Requirements Letters from Public Works dated January 15, 2010, Fire dated September 10, 2009, and Building and Safety dated September 10, 2009 (for informational purposes only)
7. Draft Negative Declaration No. 09-006



Project Site

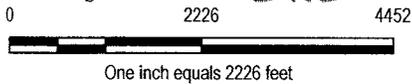


Vicinity Map

Map produced by information contained in the City of Huntington Beach Information Services Department Geographic Information System. Information warranted for City use only. Huntington Beach does not guarantee its completeness or accuracy.
Map Produced on 12/8/2009



HB
GIS



MINOR NAMES

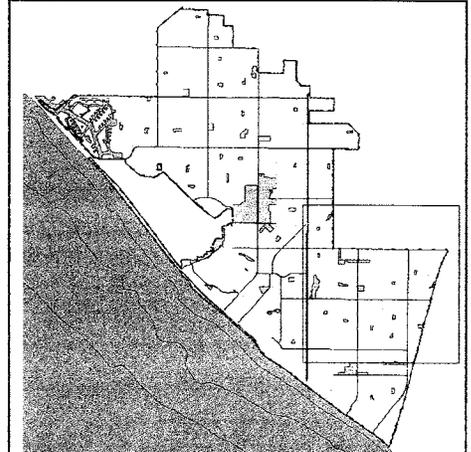
~ CITY BOUNDARY

STREET CENTERLINES (OCTAClass)

- ~ Smartstreet
- ~ Major
- ~ Collector
- ~ Primary
- ~ Secondary
- ~ Residential
- ~ Travelway
- ~ Alley

ISOBATHS

~ HARBOR





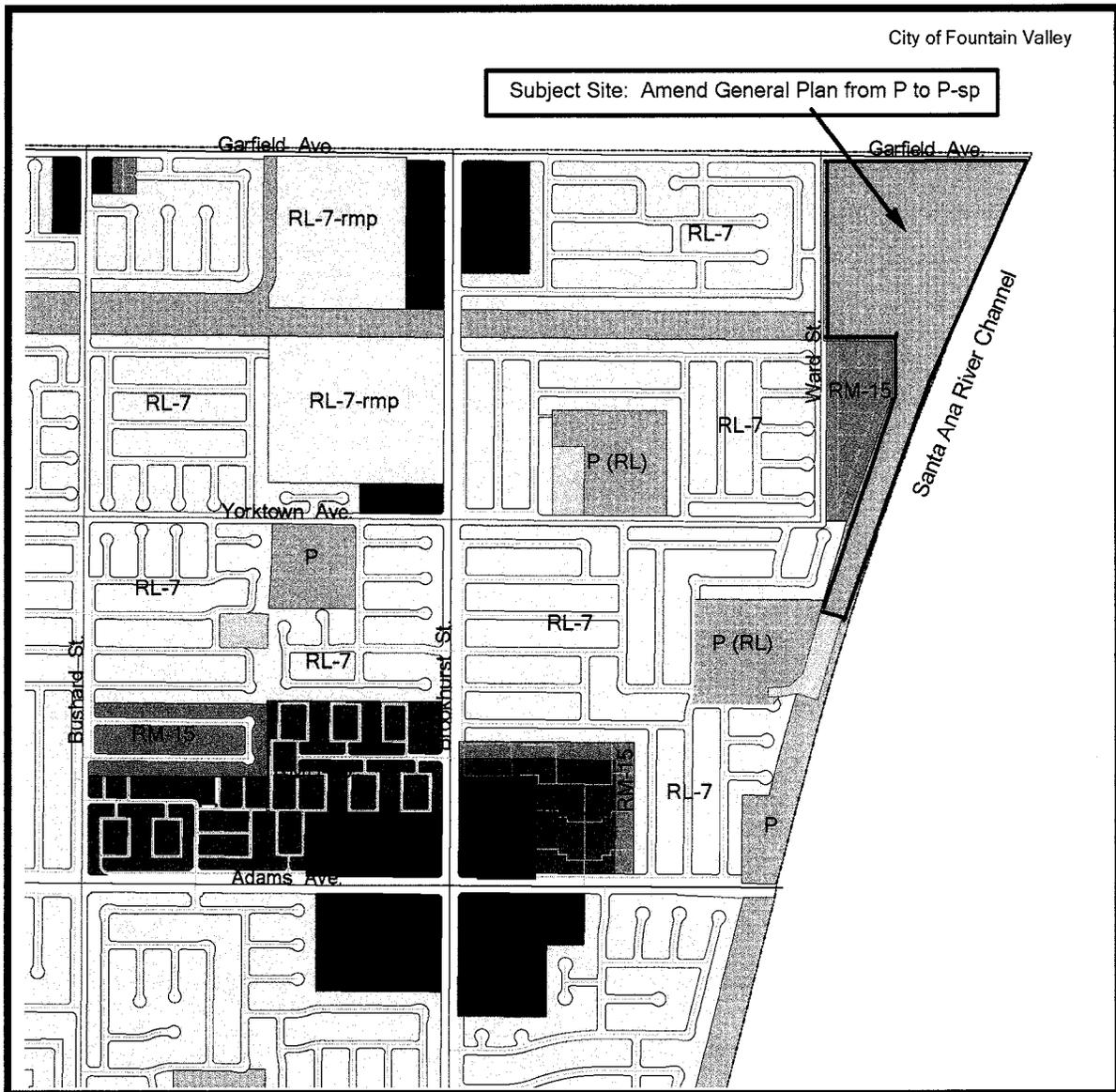
SITE AERIAL (SA1)

SITE PHOTO'S

WARD GARFIELD SPECIFIC PLAN
 HUNTINGTON BEACH RV YARD
 19118 WARD ST, HUNTINGTON BEACH, CA 92646

PC6
 ATTACHMENT NO. 1.2

access design group

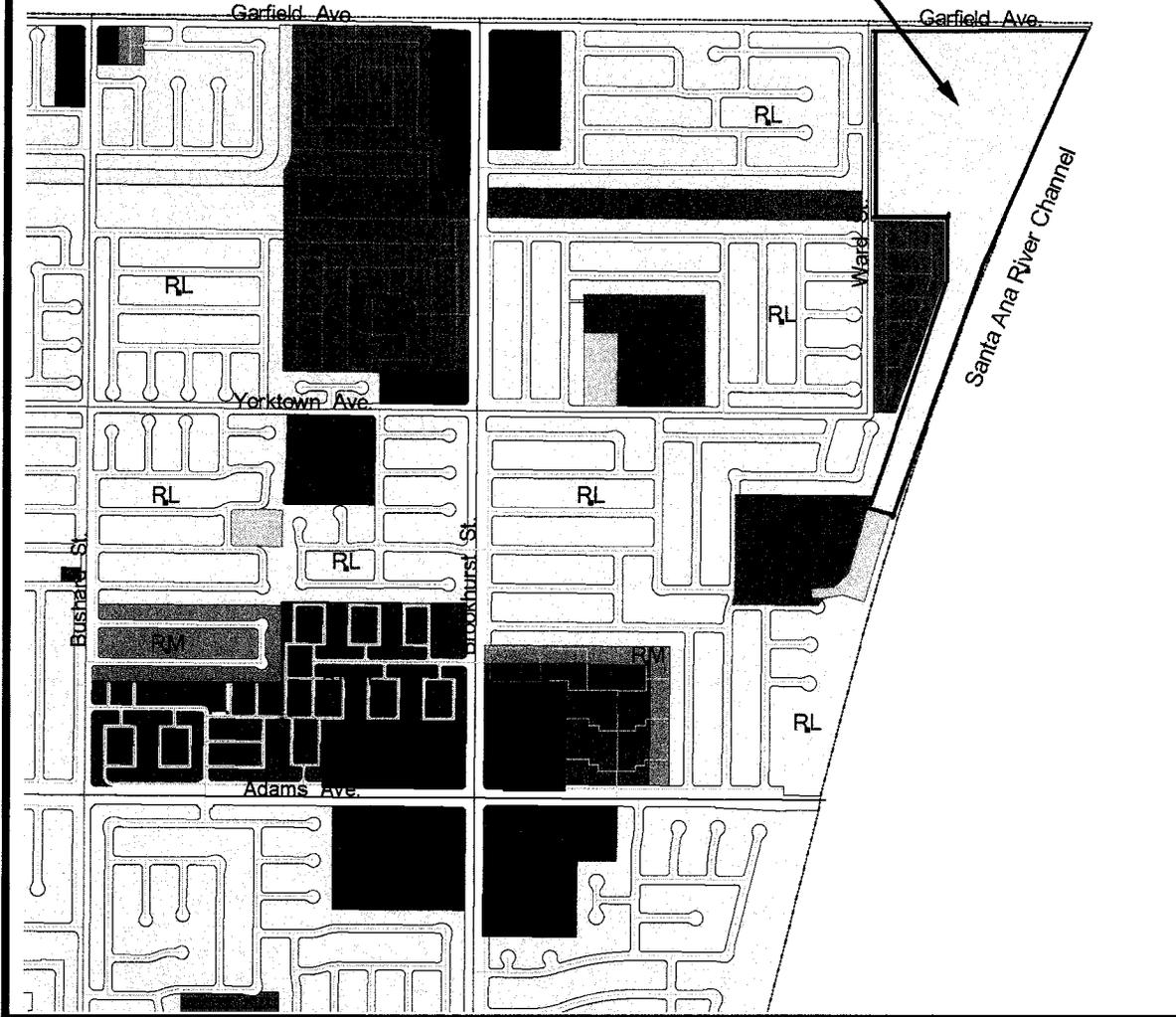


CURRENT AND PROPOSED GENERAL PLAN DESIGNATIONS

**GENERAL PLAN AMENDMENT NO. 09-002, ZONING MAP AMENDMENT
NO. 09-002, ZONING TEXT AMENDMENT NO. 09-007,
CONDITIONAL USE PERMIT NO. 09-024, NEGATIVE
DECLARATION NO. 09-006**

(WARD GARFIELD SPECIFIC PLAN/RECREATIONAL VEHICLE STORAGE)

Subject Site: Amend Zoning from RL to Ward Garfield Specific Plan No. 16



CURRENT AND PROPOSED ZONING DESIGNATIONS

**GENERAL PLAN AMENDMENT NO. 09-002, ZONING MAP AMENDMENT
NO. 09-002, ZONING TEXT AMENDMENT NO. 09-007,
CONDITIONAL USE PERMIT NO. 09-024, NEGATIVE
DECLARATION NO. 09-006
(WARD GARFIELD SPECIFIC PLAN/RECREATIONAL VEHICLE STORAGE)**

NARRATIVE

LOCATION: 19118 Ward Street
(Southeast corner of Ward St. and Garfield Avenue)

REQUEST: GPA: To add a Specific Plan suffix to the existing Public (P) designation
ZMA: To change the RL zoning designation to the Ward Garfield Specific Plan
ZTA: To adopt the Ward Garfield Specific Plan
CUP: To establish a 557 space RV Storage Facility on approx. 13.52 acres.

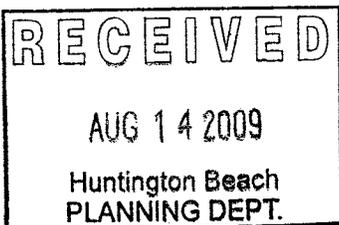
EXISTING CONDITIONS: The project site consists of approximately 43.60 acres at the southeast corner of Ward Street and Garfield Avenue at the east end of the city. The site is owned and operated by Southern California Edison who currently leases portions of the property to landscape nursery uses. The Ellis Substation which occupies approximately 13 acres is centrally located on the site. No changes to the substation are proposed. Existing improvements include an approximately 2000 sq. ft. substation building with an ancillary service truck bay, an approximately 500 sq. ft. landscape nursery office, greenhouse structures, electrical transmission utilities, and paved surfaces for parking and access. Two driveways provide access to the site via Ward Street and one driveway off Garfield Avenue.

PROPOSED PROJECT: The applicant is requesting a General Plan Amendment and Zoning Map Amendment to adopt a new specific plan allowing for the establishment of an RV Storage facility on approximately 13.52 acres at the south end of the project site. The area also referred to as "the 7" due to its lot configuration, will consist of 557± RV parking spaces, an 800 sq. ft. modular rental office with restroom, and associated perimeter fencing and lighting. There will be a washing area, dump station, and trash enclosure inside the facility. Existing transmission towers and electrical lines will continue to exist above the RV parking facility.

The new RV Storage facility is designed with access from Ward Street with a large queuing area to an automated remote gate system. Approximately 8.8% of the site would be landscaped with the majority of the landscaping installed in more visible areas at the front entry and within the proposed ornamental fencing abutting residential properties.

The proposed RV Storage hours of operation:
Sunday – Friday: 8AM-5PM
Saturday 7AM-6PM

There will one daytime office manager and 2-4 employees that work throughout the day.



ZONING AND
GENERAL PLAN:

Existing zoning: RL
Proposed zoning: Ward Garfield Specific Plan
Existing General Plan designation: P (Public)
Proposed General Plan: P-sp (Public – specific plan)

ENVIRONMENTAL STATUS:

A draft Environmental Checklist has been completed and submitted along with this request.

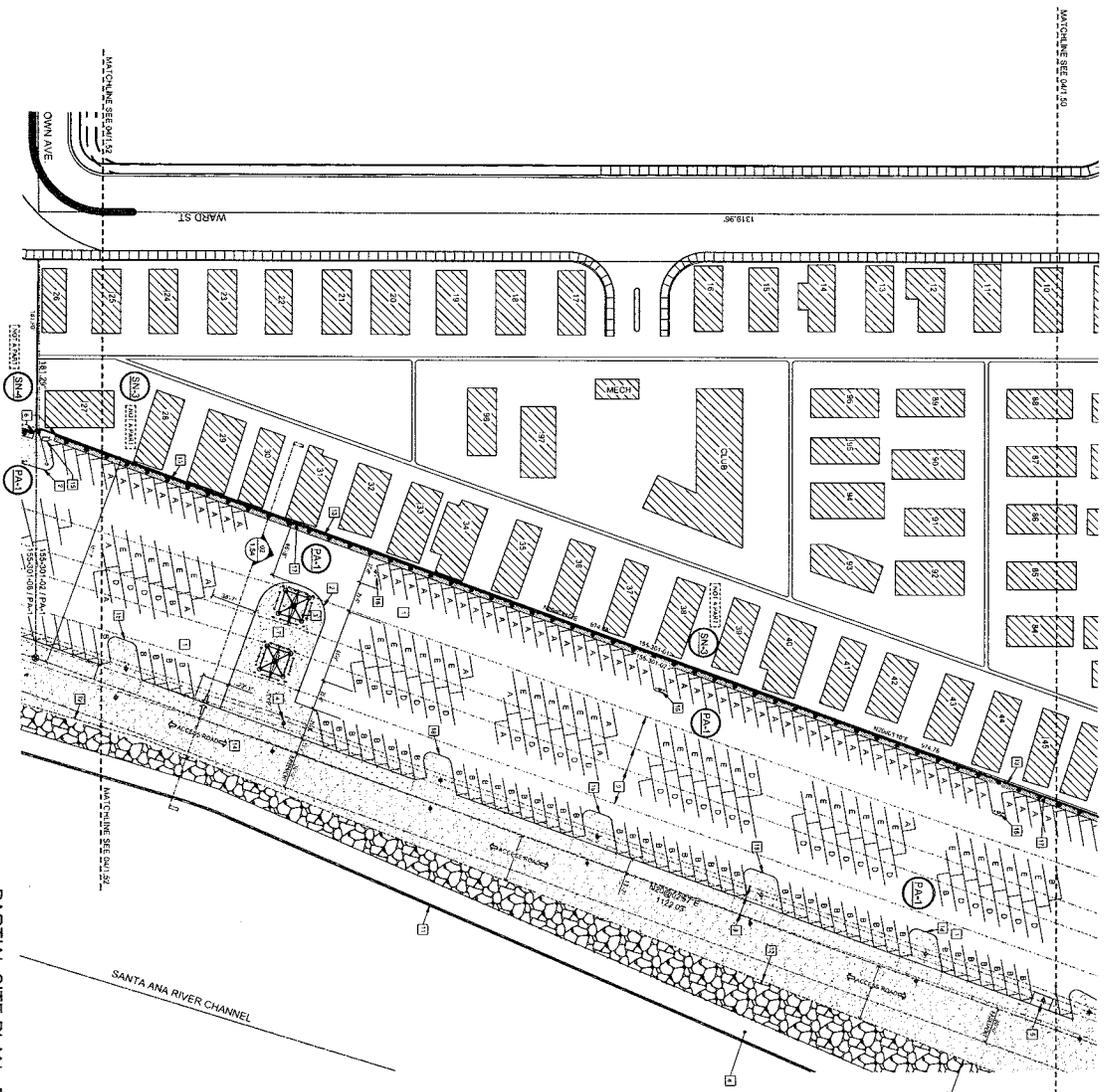
PROJECT AREAS:

Total Area (entire Specific Plan):	43.60 AC
Total Area (Planning Area 1):	13.52 AC
Total Area (Planning Area 2):	12.95 AC
Total Area (Planning Area 3):	17.13 AC

SURROUNDING USES:

North – OCSD (Fountain Valley)
East – Santa Ana River Channel (OCFCD)
West – Mobile home park/Single family residences
South – Mobile home park/Arevalos Park

August 12, 2009



PARTIAL SITE PLAN - PLANNING AREA PA-1

04

LEGEND

- 1. BUILDING FOOTPRINT
- 2. EXISTING PAVEMENT
- 3. EXISTING SIDEWALK
- 4. EXISTING DRIVEWAY
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PART SITE PLAN NOTES

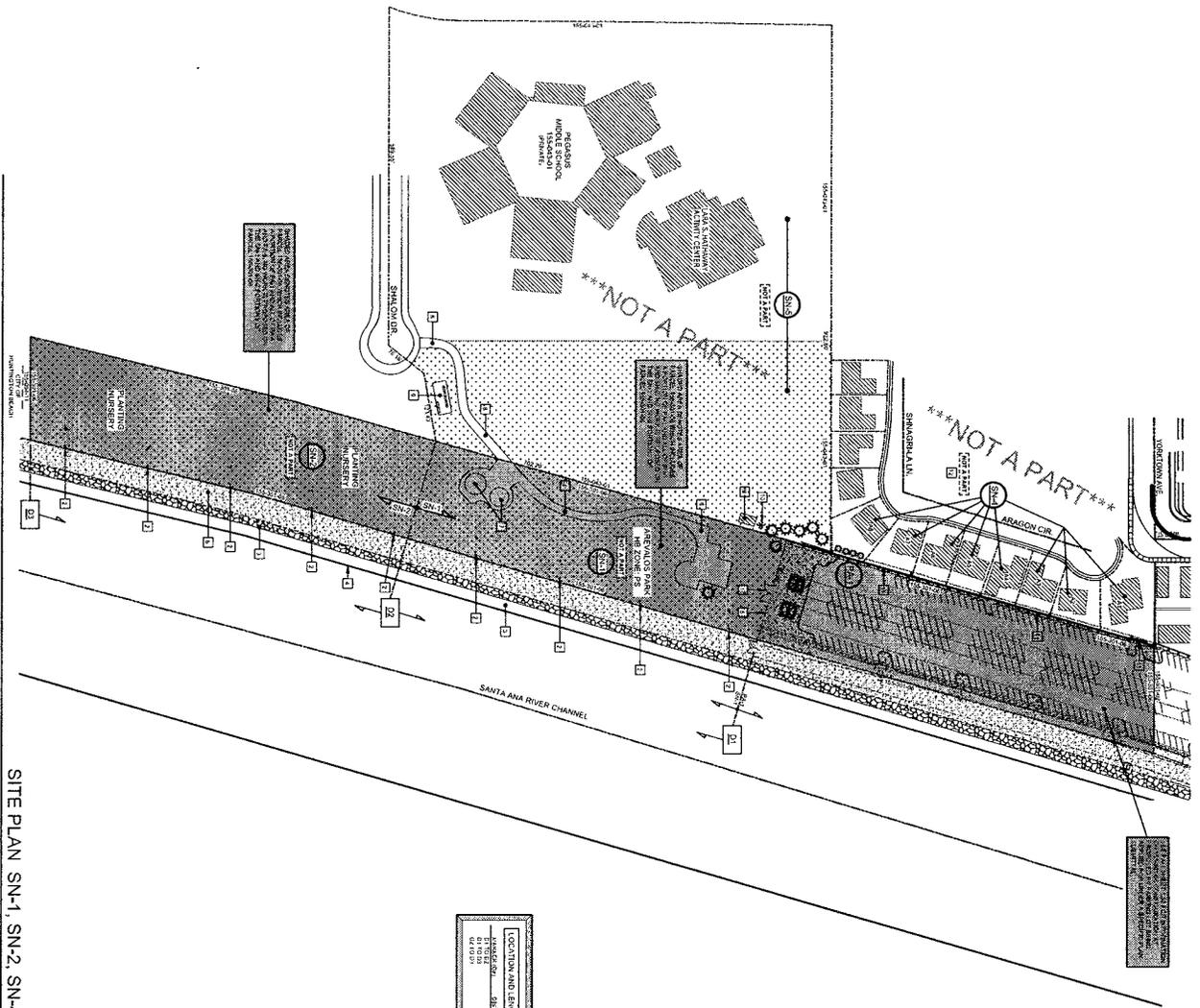
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PARKING STALL LEGEND

A - 514' x 10' x 10'

ward garfield specific plan - specific plan #16
 huntington beach - tv storage facility

19118 ward street, huntington beach, california 92646
 for : Southern California Edison, 14799 Chestnut Street, Westminster, Ca, 92683
 access design group 18600 main street, suite 110, huntington beach, california 92648-0006 714.841-7147



ward garfield specific plan - specific plan #16
 huntington beach - rv storage facility

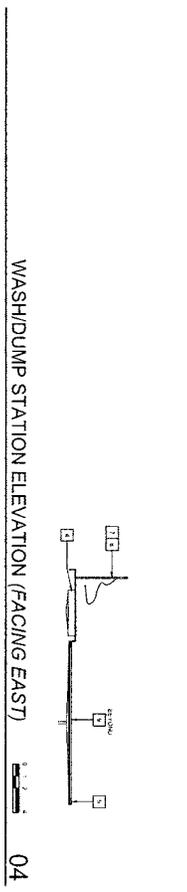
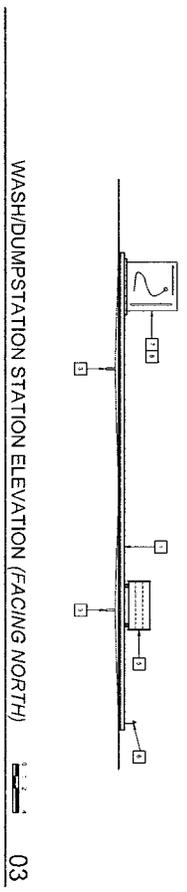
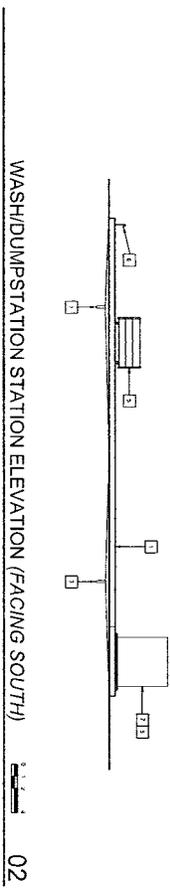
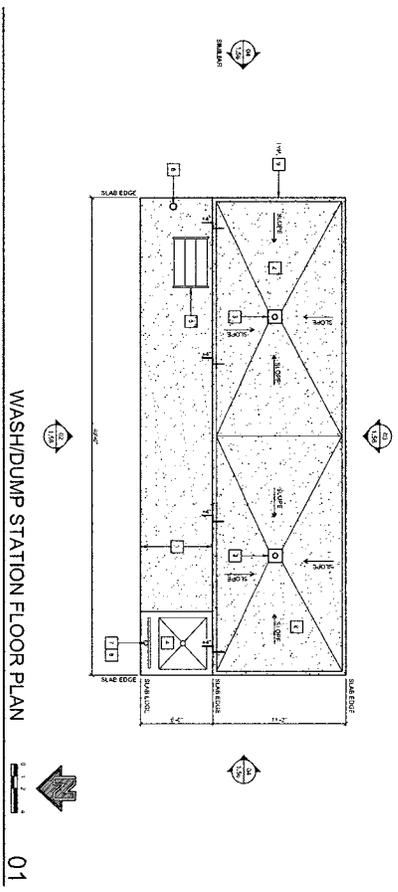
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 access design group 18600 Main Street, Suite 110, Huntington Beach, California 92648-6006 714.841-7147

SITE PLAN SN-1, SN-2, SN-4 & SN-5



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<p>GENERAL NOTES</p> <p>1. ALL UTILITIES SHOWN ARE BASED ON RECORD DRAWINGS AND FIELD SURVEY. VERIFY ALL UTILITIES PRIOR TO CONSTRUCTION.</p> <p>2. THE OWNER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>3. THE DESIGNER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p>	
<p>LEGEND</p> <p>1. RECREATION MIDDLE SCHOOL</p> <p>2. HUNTINGTON BEACH RV STORAGE FACILITY</p> <p>3. SANTA ANA RIVER CHANNEL</p> <p>4. ACCESS POINTS</p> <p>5. UTILITY LOCATIONS</p> <p>6. EXISTING UTILITIES</p> <p>7. PROPOSED UTILITIES</p> <p>8. EXISTING DRIVEWAYS</p> <p>9. PROPOSED DRIVEWAYS</p> <p>10. EXISTING SIDEWALKS</p> <p>11. PROPOSED SIDEWALKS</p> <p>12. EXISTING PAVEMENT</p> <p>13. PROPOSED PAVEMENT</p> <p>14. EXISTING GRASS</p> <p>15. PROPOSED GRASS</p> <p>16. EXISTING TREES</p> <p>17. PROPOSED TREES</p> <p>18. EXISTING FENCES</p> <p>19. PROPOSED FENCES</p> <p>20. EXISTING LIGHTS</p> <p>21. PROPOSED LIGHTS</p> <p>22. EXISTING SIGNAGE</p> <p>23. PROPOSED SIGNAGE</p> <p>24. EXISTING LANDSCAPE</p> <p>25. PROPOSED LANDSCAPE</p>	
<p>SITE PLAN NOTES</p> <p>1. THE SITE PLAN IS SUBJECT TO ALL APPLICABLE REGULATIONS AND ORDINANCES OF THE LOCAL AND STATE AGENCIES.</p> <p>2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p> <p>10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AND STATE AGENCIES.</p>	



- WASH STATION PLAN NOTES**
- 1. CONCRETE SLAB ON GRADE - 8" THICK WITH REINFORCING
 - 2. 2" FILL PLASTER OVER THE SLAB. DIMENSIONS TO CENTERLINE UNLESS NOTED OTHERWISE
 - 3. ALL DIMENSIONS UNLESS NOTED OTHERWISE
 - 4. ALL DIMENSIONS UNLESS NOTED OTHERWISE
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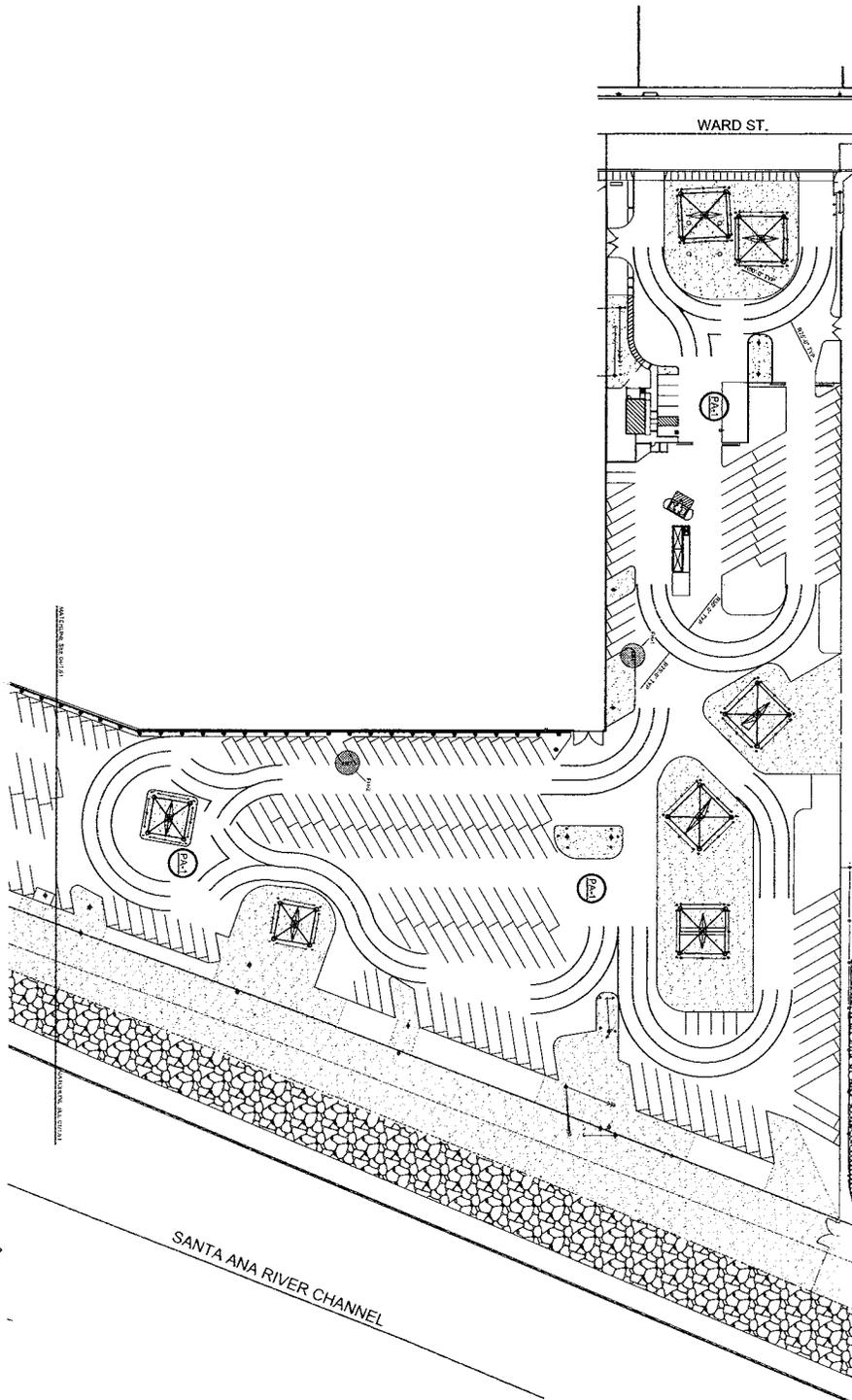
ward garfield specific plan #16
 huntington beach - tv storage facility

19118 ward street, huntington beach, california 92646
 for: Southern California Edison, 14799 Chestnut Street, Westminster, Ca. 92683
 access design group 18800 main street, suite 110, huntington beach, california 92648-8008 714.841-7147



DATE: 02/20/12
 DRAWN BY: J. L. LEE
 CHECKED BY: J. L. LEE
 SCALE: AS SHOWN

SHEET
 1.56
 HD-11-0807-2



PARTIAL FIRE DEPT. TURNING RADIUS SITE PLAN PA-1

04

ward garfield specific plan - specific plan #16
 huntington beach - tv storage facility

19118 ward street, huntington beach, california 92646
 for : Southern California Edison, 14799 Chestnut Street, Westminster, Ca, 92683
 access design group 16600 main street, suite 110, huntington beach, california 92646-6006 714.861-7147

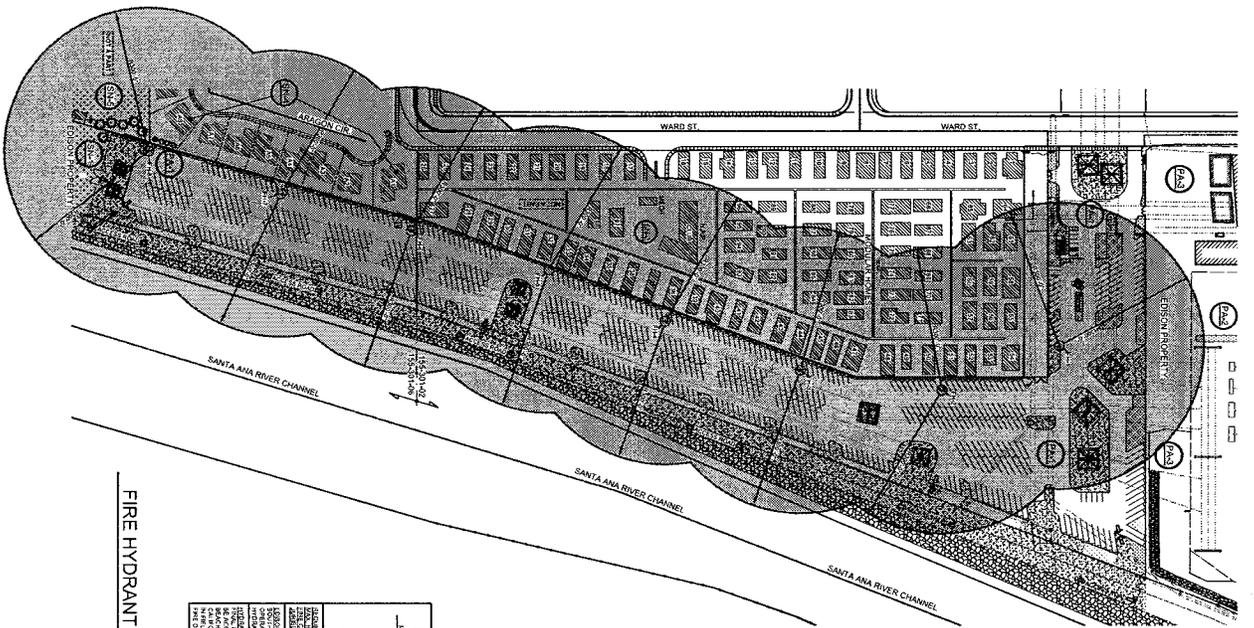
DATE: 11/19/2007 11:41 AM PROJECT NO: 11617 PROJECT NAME: Ward Garfield Specific Plan #16 - TV Storage Facility SHEET NO: 04 OF 04

ATTACHMENT NO. S.10



DATE: 11/19/2007
 DRAWN BY: J. S. SMITH
 CHECKED BY: J. S. SMITH
 DESIGNED BY: J. S. SMITH
 PROJECT NO: 11617
 SHEET NO: 04 OF 04

1.60
 11/19/2007



FIRE HYDRANT SPACING	
Maximum Spacing	300 FT
Minimum Spacing	150 FT
Maximum Spacing	300 FT
Minimum Spacing	150 FT
Maximum Spacing	300 FT
Minimum Spacing	150 FT

FIRE HYDRANT PLAN



04

ward garfield specific plan - specific plan #16
 huntington beach - tv storage facility

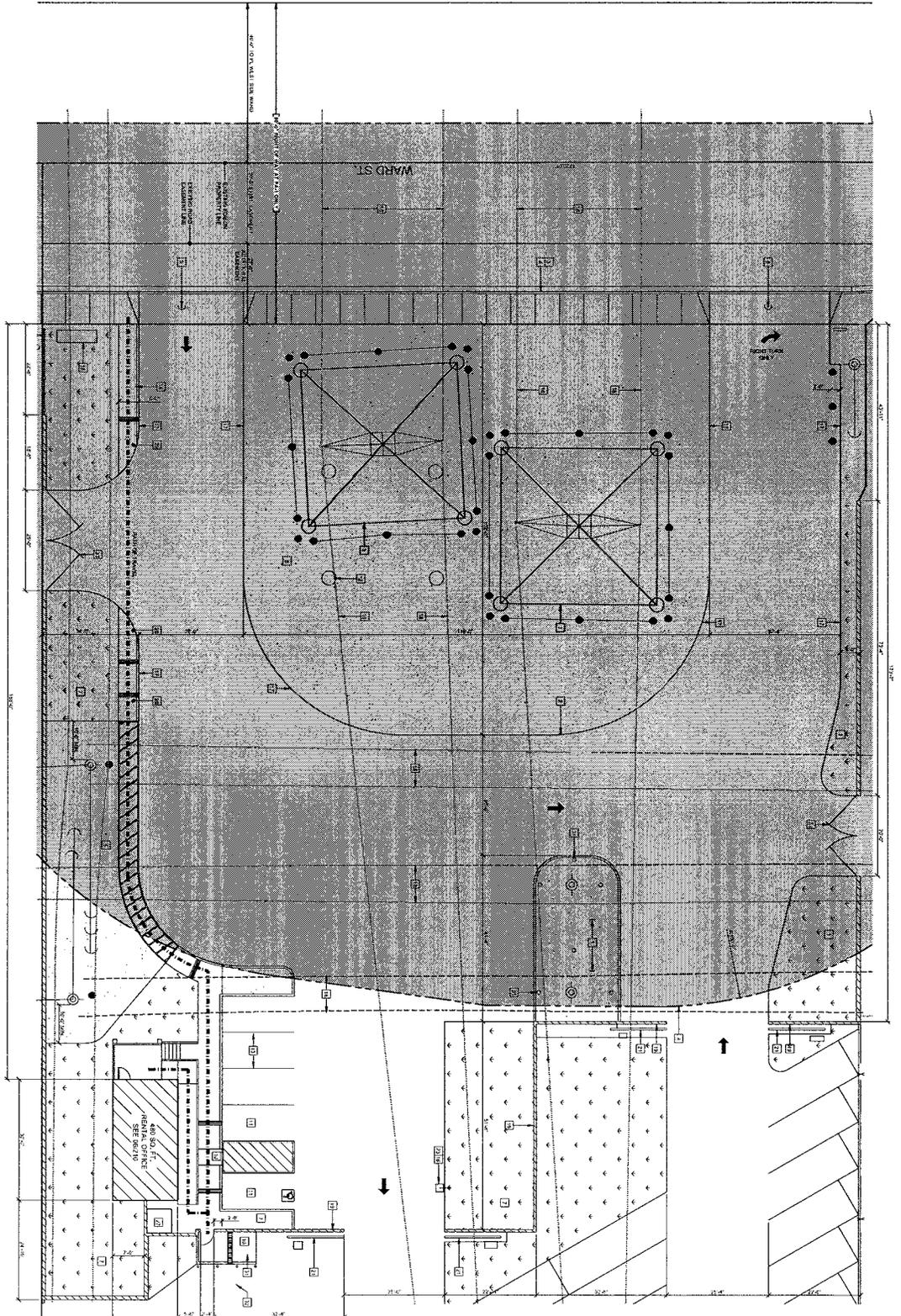
19718 Ward Street, Huntington Beach, California 92646
 for: Southern California Edison, 14799 Chestnut Street, Westminster, Ca, 92683

access design group 18800 main street, suite 110, Huntington Beach, California 92648-6006 714.841.7147

DATE: 08/11/09
 DRAWN BY: BLANK, SHERIDAN
 CHECKED BY: THOMAS, SHERIDAN
 DESIGNED BY: THOMAS, SHERIDAN
 PROJECT NO.: 04101110
 SHEET NO.: 04



SHEET
 1.70
 PG. 11 OF 04/13

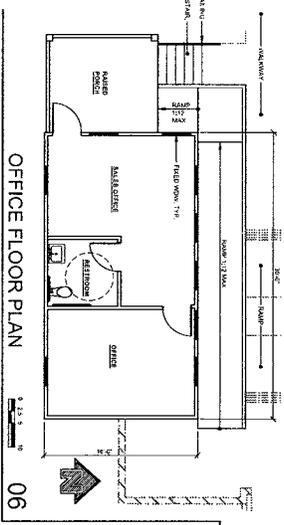


- SITE PLAN NOTES**
1. LUMP SUM
 2. INCLUDE COSTS FOR PERMITS, FEES, AND INSURANCE
 3. PROVIDE ALL NECESSARY UTILITY INFORMATION AND RECORDS
 4. PROVIDE ALL NECESSARY CONSTRUCTION INFORMATION
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 100. PROVIDE ALL NECESSARY CONSTRUCTION INFORMATION

ENLARGED SITE PLAN



04



OFFICE FLOOR PLAN

06

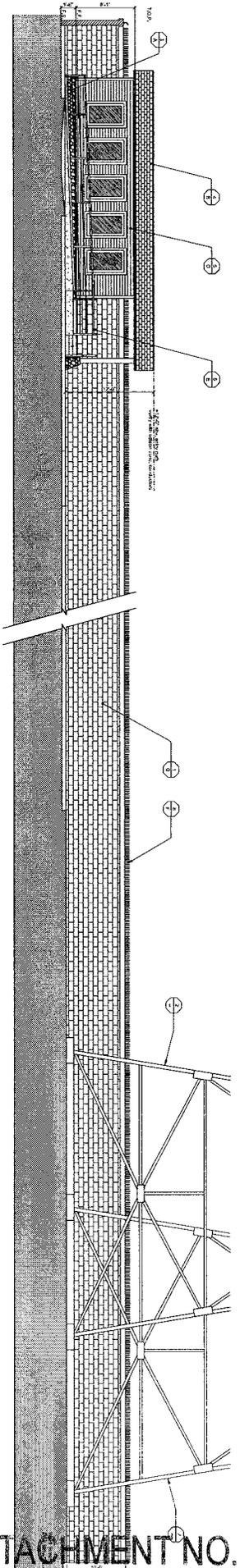
ward garfield specific plan - specific plan #16
huntington beach - rv storage facility

19718 Ward Street, Huntington Beach, California 92646
 for: Southern California Edison, 54799 Chestnut Street, Westminster, Ca, 92683
 access design group 18800 main street, suite 110, Huntington Beach, California 92648-8006 714.841-7147

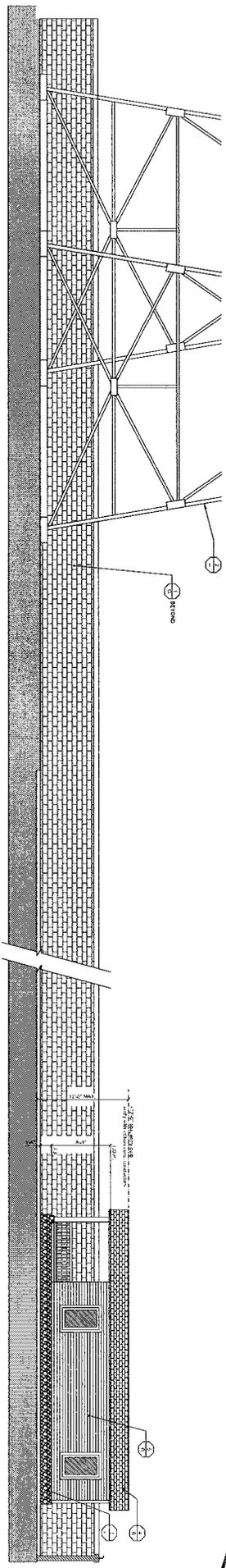
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- DESIGN: 08/10/12
- REVISION: 08/10/12
- SCALE: 1/8" = 1'-0"
- PROJECT: 16-0001
- CLIENT: SOUTHERN CALIFORNIA EDISON



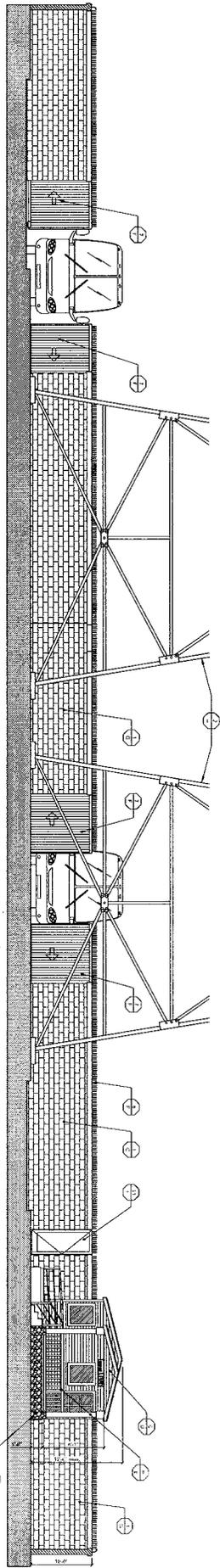
SHEET
2.110
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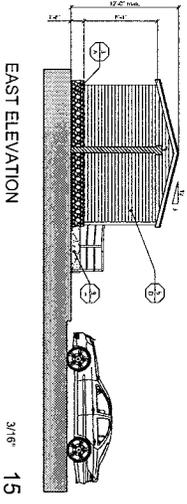
NORTH ELEVATION 3/16" 02



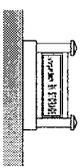
SOUTH ELEVATION 3/16" 03



WEST ELEVATION 3/16" 04



EAST ELEVATION 3/16" 15



MONUMENT SIGN 3/16" 05

ALL SYMBOLS UNLESS SPECIFIED OTHERWISE

MATERIALS & FINISHES	ALL MATERIALS TO BE NON-FLAMMABLE
① BRICK	① BRICK
② BRICK	② BRICK
③ BRICK	③ BRICK
④ BRICK	④ BRICK
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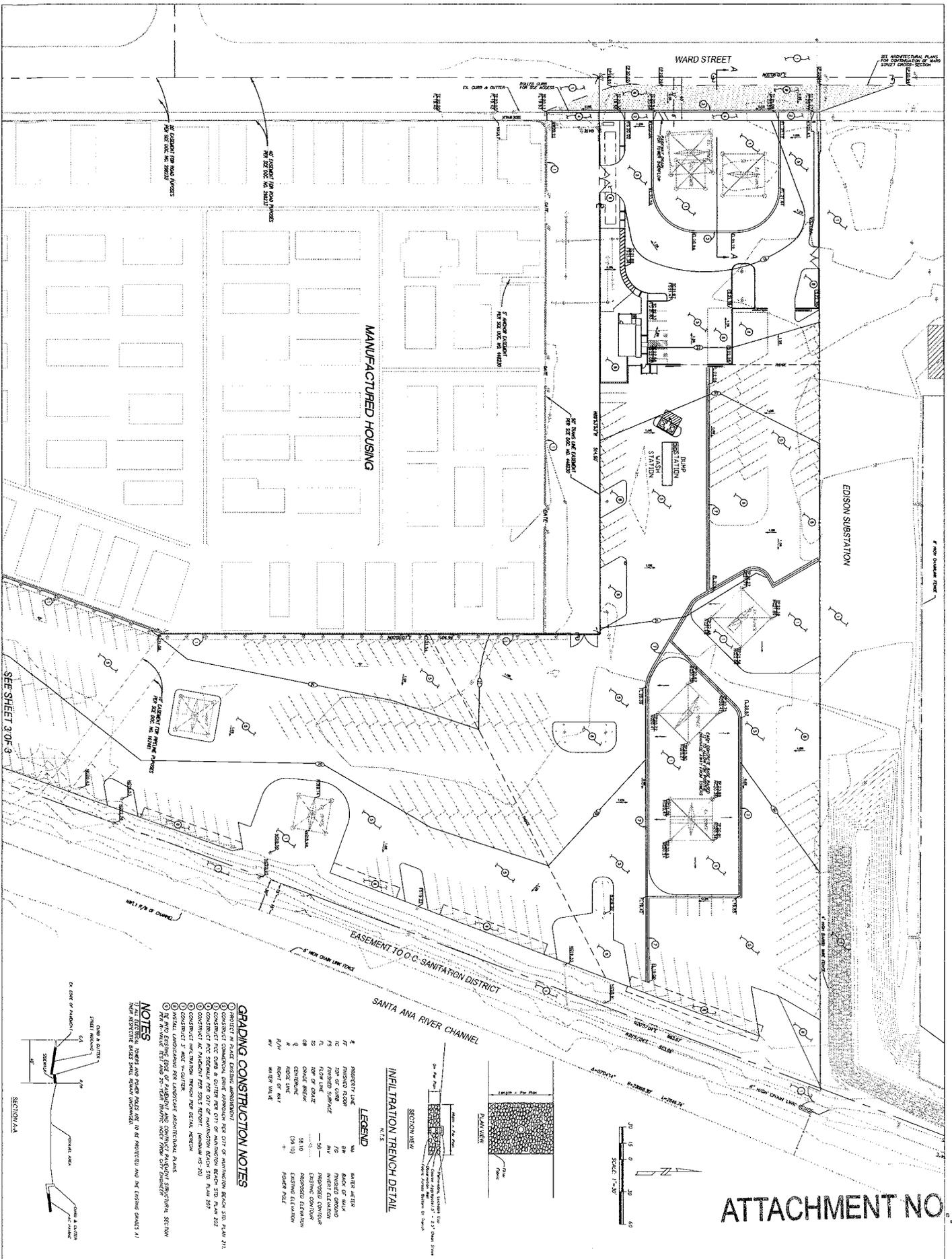
ward garfield specific plan - specific plan #16
 huntington beach - tv storage facility

access design group 18800 main street, suite 110, huntington beach, california 92648-6008 714.841.7147
 for: Southern California Edison, 14799 Chestnut Street, Westminster, Ca, 92683



DATE: 08/11/10
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 TITLE: [Title]
 PROJECT: [Project Name]

3.10
 SHEET

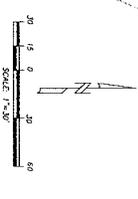
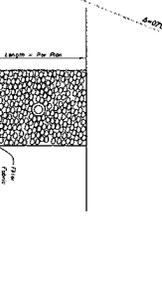
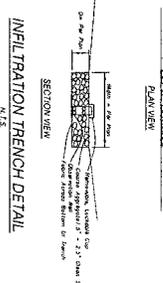


ATTACHMENT NO. 5.17

- GRADING CONSTRUCTION NOTES**
1. PROJECT IN A-1 CLASSIFICATION.
 2. CONSTRUCT COMPLETE DRAINAGE SYSTEM PER CITY OF HUNTINGTON BEACH STD. 14.14.1.1.
 3. CONSTRUCT 12" DIA. 12' DEEP MANHOLE PER CITY OF HUNTINGTON BEACH STD. 14.14.1.1.
 4. CONSTRUCT 12" DIA. 12' DEEP MANHOLE PER CITY OF HUNTINGTON BEACH STD. 14.14.1.1.
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 9. CONSTRUCT 12" DIA. 12' DEEP MANHOLE PER CITY OF HUNTINGTON BEACH STD. 14.14.1.1.
 10. CONSTRUCT 12" DIA. 12' DEEP MANHOLE PER CITY OF HUNTINGTON BEACH STD. 14.14.1.1.

LEGEND

1/2" DIA. 12' DEEP MANHOLE	12" DIA. 12' DEEP MANHOLE	12" DIA. 12' DEEP MANHOLE	12" DIA. 12' DEEP MANHOLE
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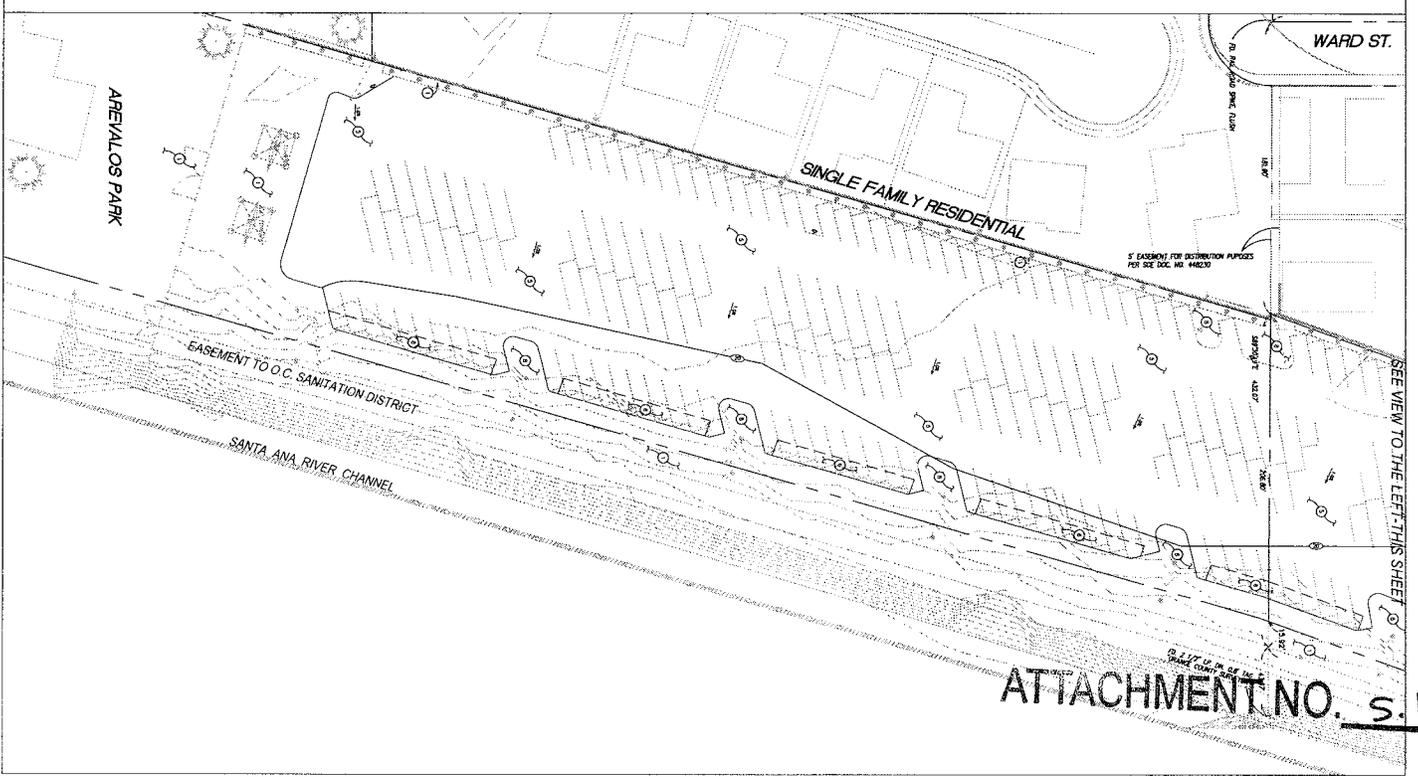
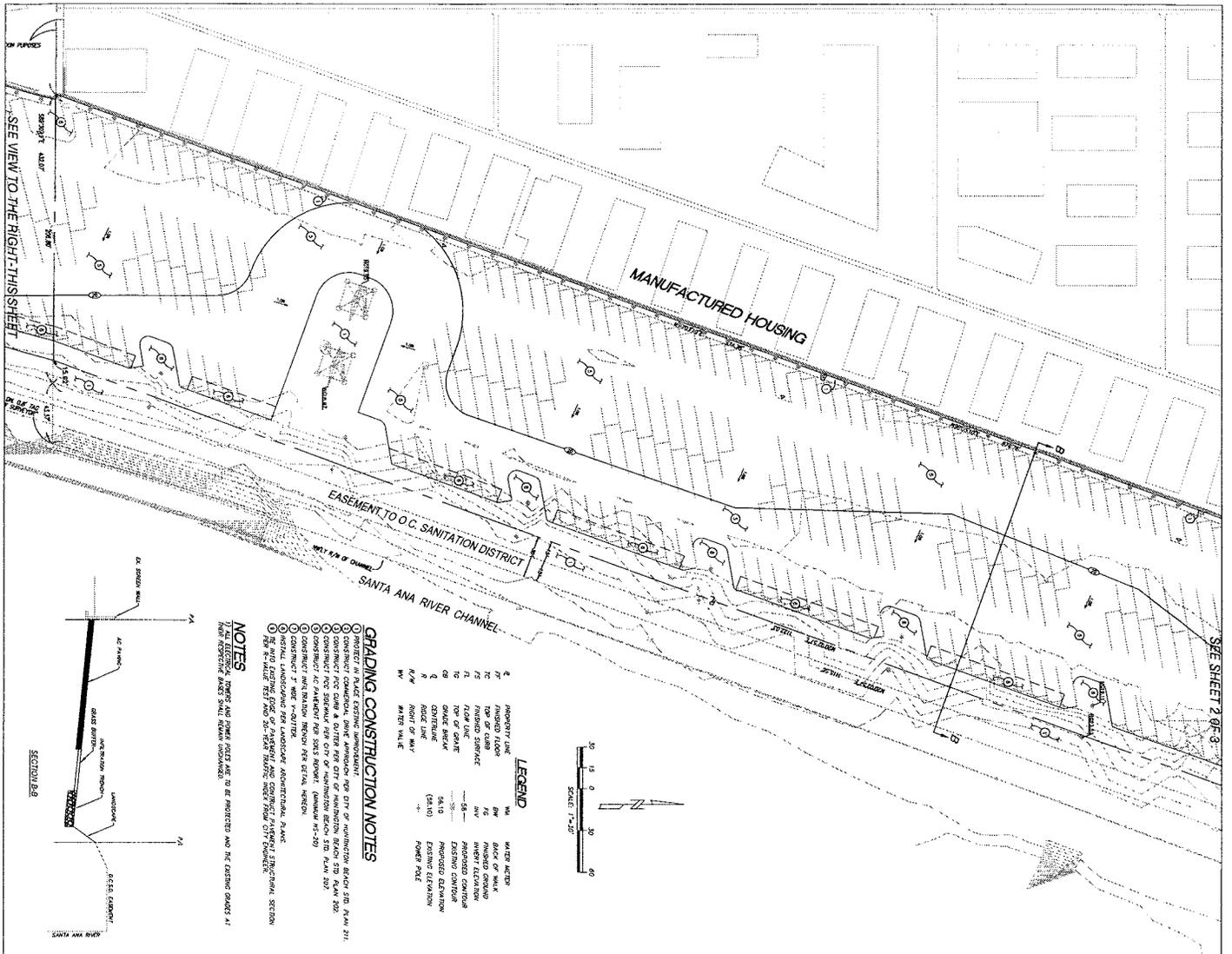
HUNTINGTON BEACH RV STORAGE
 13116 WARD STREET
 HUNTINGTON BEACH, CA 92646

BLUE PEAK ENGINEERING, INC.
 7450 EAST WELLS STREET
 ANAHEIM, CA 92808
 714.748.3077
 714.281.1840 FAX

C-2

PRELIMINARY GRADING PLAN

The seal is a circular emblem containing the text 'BLUE PEAK ENGINEERING, INC.' and 'REGISTERED PROFESSIONAL ENGINEER'. It is used to certify the accuracy and compliance of the engineering plan.



ATTACHMENT NO. 5.10

DAVID J. SMITH

 CIVIL ENGINEER

 STATE OF CALIFORNIA

 No. 100048

 EXPIRES 12/31/2024

HUNTINGTON BEACH RV STORAGE

 19116 WARD STREET

 HUNTINGTON BEACH, CA 92646

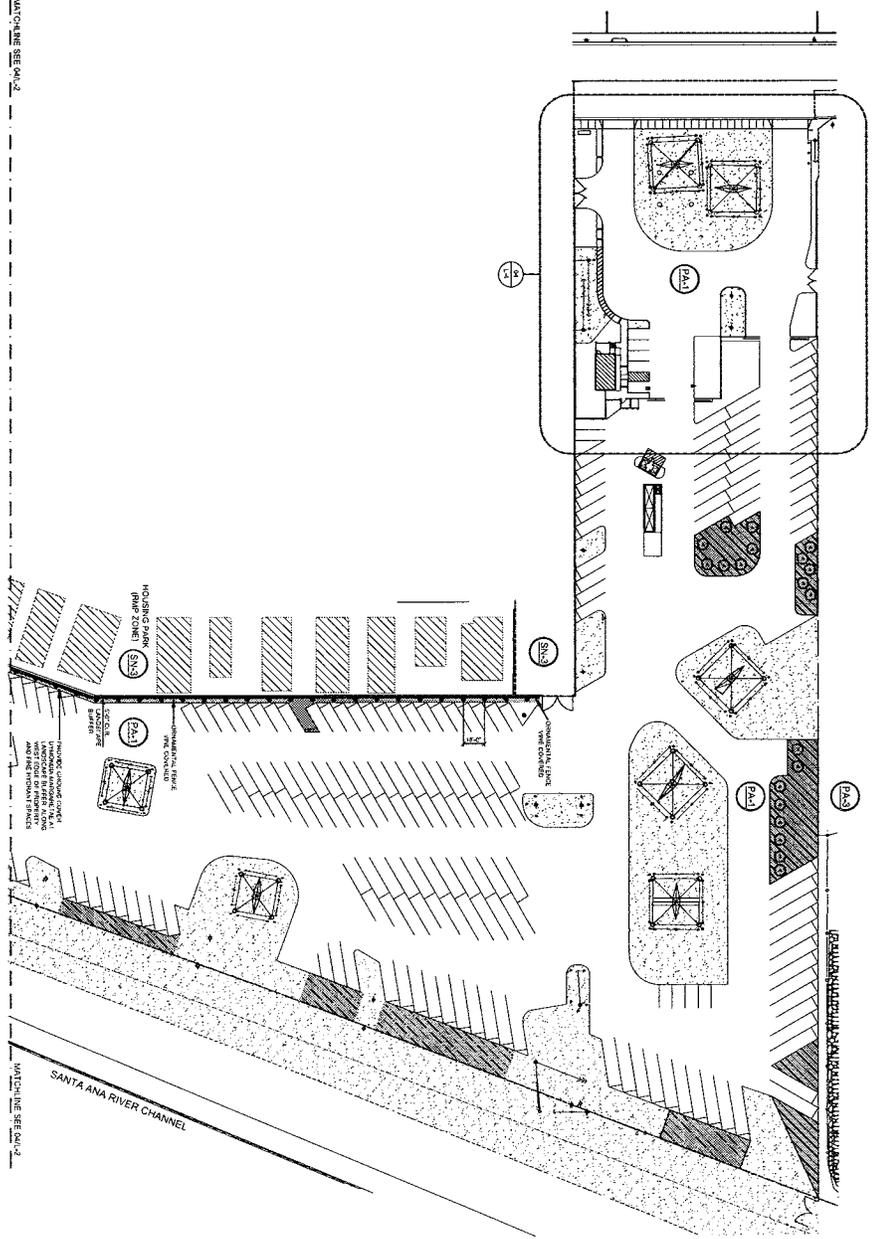
BLUE PEAK ENGINEERING, INC.

 1400 EAST MESA STREAM CTR.

 ANAHEIM, CA 92804

 714.748.3077

 714.281.1840 FAX



LEGEND

EXISTING PLANT SPECIFICATIONS
 PLANT SPECIFICATIONS
 PLANT SPECIFICATIONS
 PLANT SPECIFICATIONS

PLANT PALLETTE

TREES

- 1. 12" DBH. 20' H. 10" CAL. 10" CAL.
- 2. 12" DBH. 20' H. 10" CAL. 10" CAL.
- 3. 12" DBH. 20' H. 10" CAL. 10" CAL.
- 4. 12" DBH. 20' H. 10" CAL. 10" CAL.

SHRUBS/VINES

- 1. 12" DBH. 20' H. 10" CAL. 10" CAL.
- 2. 12" DBH. 20' H. 10" CAL. 10" CAL.
- 3. 12" DBH. 20' H. 10" CAL. 10" CAL.
- 4. 12" DBH. 20' H. 10" CAL. 10" CAL.

NOTES:

1. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CALIFORNIA PLANTING STANDARDS.
2. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CALIFORNIA PLANTING STANDARDS.
3. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CALIFORNIA PLANTING STANDARDS.
4. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CALIFORNIA PLANTING STANDARDS.

PARTIAL LANDSCAPE PLAN - PLANNING AREA PA-1

ward garfield specific plan - specific plan #16
 huntington beach - rv storage facility

19118 Ward Street, Huntington Beach, California 92646
 for: Southern California Edison, 14799 Chastnut Street, Westminster, CA, 92683
 access design group, 18800 Main Street, Suite 110, Huntington Beach, California 92648-6008 714.841.7147

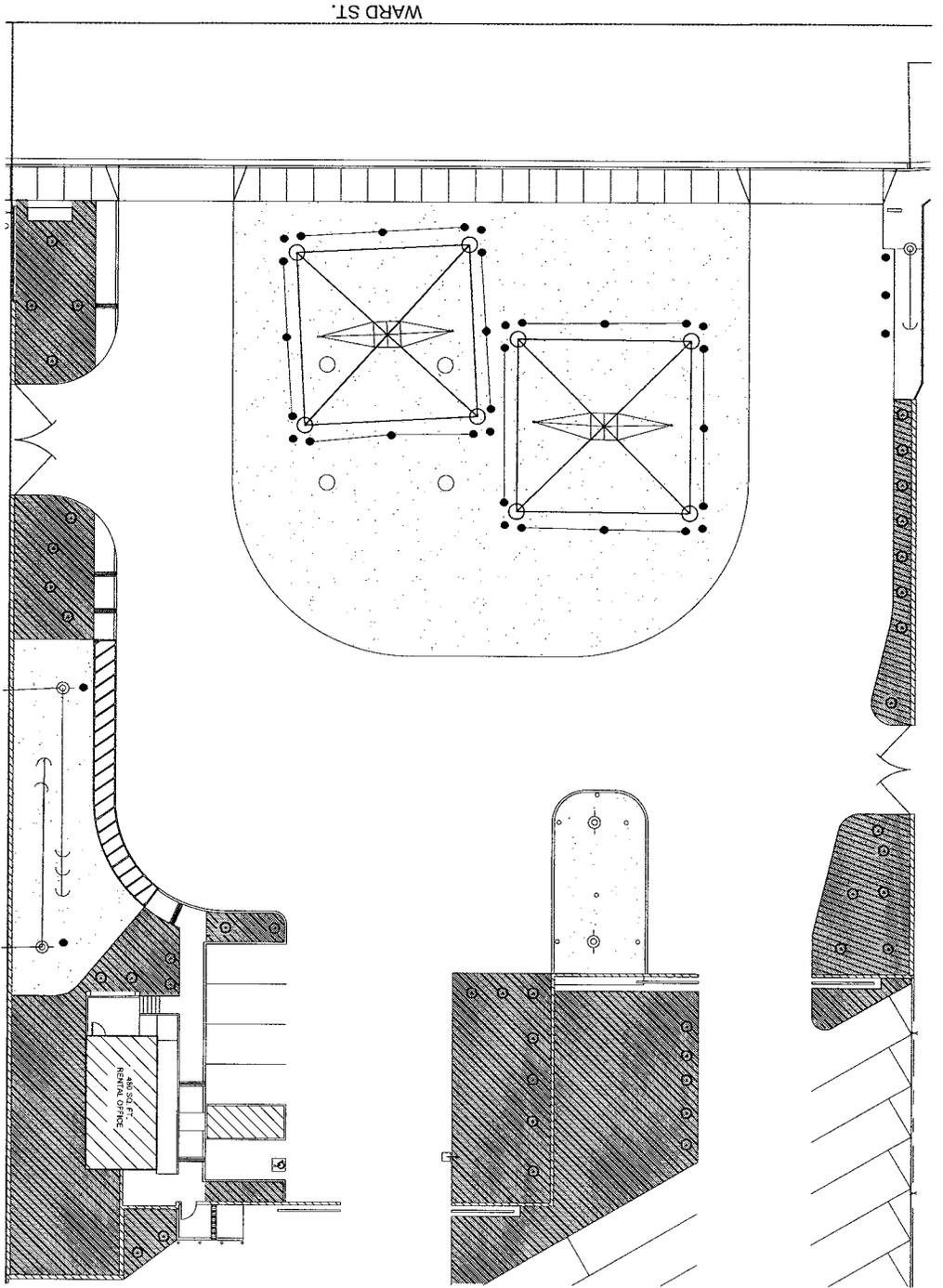
04



NO. 1000	PLANT SPECIFICATIONS
NO. 2000	LANDSCAPE SPECIFICATIONS
NO. 3000	PLANNING SPECIFICATIONS
NO. 4000	PLANNING SPECIFICATIONS
NO. 5000	PLANNING SPECIFICATIONS

SHEET
 L-1
 10-17-2012

SCALE: 1/8" = 1'-0" (VERTICAL SCALE) 1/4" = 1'-0" (HORIZONTAL SCALE) DATE: 10/17/12
 ALL RIGHTS RESERVED. THIS DOCUMENT IS THE PROPERTY OF ACCESS DESIGN GROUP AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF ACCESS DESIGN GROUP.



ENLARGED PRELIMINARY LANDSCAPE PLAN @ ENTRY

04



ward garfield specific plan - specific plan #16
 huntington beach - tv storage facility

19118 ward street, huntington beach, california 92646
 for : southern california Edison, 14799 Chestnut Street, Westminster, Ca., 92683
 access design group 18600 main street, suite 110, huntington beach, california 92648-6006 714.841-7147

LEGEND

PLANT PALETTE

TREES

SHRUBS/WINES

NOTES: ALL PLANTS TO BE INSTALLED IN THE QUARTERS OF THE SITE... (text partially obscured)

ATTACHMENT NO. 5.22

04/17/16 10:41 AM C:\Users\akdesign\Documents\Projects\161616\161616.dwg Plot Date: 04/17/16 10:41 AM Plot Scale: 1/8" = 1'-0" Plot Size: 11.00 x 17.00 inches Plot Orientation: Landscape Plot Title: 04 - ENLARGED PRELIMINARY LANDSCAPE PLAN @ ENTRY



CITY OF HUNTINGTON BEACH

PUBLIC WORKS DEPARTMENT

PROJECT IMPLEMENTATION CODE REQUIREMENTS

DATE: JANUARY 15, 2010

PROJECT NAME: RV STORAGE FACILITY, WARD GARFIELD SPECIFIC PLAN NO. 14

ENTITLEMENTS: GPA 09-02, ZMA 09-02, ZTA 09-07, CUP 09-24, EA 09-06

PLNG APPLICATION NO: 2009-0197

DATE OF PLANS: AUGUST 14, 2009
(DECEMBER 1, 2009 - REVISED PRELIMINARY WQMP & HYDROLOGY STUDY)

PROJECT LOCATION: 19118 WARD STREET

PROJECT PLANNER: JANE JAMES, SENIOR PLANNER

TELEPHONE/E-MAIL: 714-536-5596 / JJAMES@SURFCITY-HB.ORG

PLAN REVIEWER: BOB MILANI, SENIOR CIVIL ENGINEER *BM*

TELEPHONE/E-MAIL: 714-374-1735 / BOB.MILANI@SURFCITY-HB.ORG

PROJECT DESCRIPTION: TO PERMIT THE ESTABLISHMENT OF A NEW 557 SPACE RECREATION VEHICLE STORAGE FACILITY ON AN APPROXIMATELY 13.52 ACRE PROPERTY IN CONJUNCTION WITH THE ADOPTION OF THE WARD GARFIELD SPECIFIC PLAN FOR A 43.6 ACRE PORTION OWNED BY SOUTHERN CALIFORNIA EDISON.

The following is a list of code requirements deemed applicable to the proposed project based on plans as stated above. The items below are to meet the City of Huntington Beach's Municipal Code (HBMC), Zoning and Subdivision Ordinance (ZSO), Department of Public Works Standard Plans (Civil, Water and Landscaping) and the American Public Works Association (APWA) Standards Specifications for Public Works Construction (Green Book), the Orange County Drainage Area management Plan (DAMP), and the City Arboricultural and Landscape Standards and Specifications. The list is intended to assist the applicant by identifying requirements which shall be satisfied during the various stages of project permitting, implementation and construction. If you have any questions regarding these requirements, please contact the Plan Reviewer or Project Planner.

TENTATIVE PARCEL MAP

THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO RECORDATION OF THE FINAL PARCEL MAP:

1. A Tentative Parcel Map shall be submitted to the Planning Department for review and approval. (SMA 66424, ZSO 253.14)

ATTACHMENT NO. 6.1

2. A Final Parcel Map shall be submitted to the Department of Public Works for review and approval and shall include a title report to indicate the fee title owner(s) as shown on a title report for the subject properties. The title report shall not be more than six (6) weeks old at the time of submittal of the Final Parcel Map. (SMA 66424, ZSO 253.12)
3. The Final Parcel Map shall be consistent with the approved Tentative Parcel Map. (ZSO 253.14)
4. The following dedications to the City of Huntington Beach shall be shown on the Final Parcel Map: (ZSO 230.084A & 253.10K)
 - a. A 20-foot right-of-way dedication for public street and public utilities purposes along the entire Ward Street frontage of APN 155-301-02 per Public Works Standard Plan No. 102. (GP, ZSO 230.84)
 - b. A 20-foot right-of-way dedication for public street and public utilities purposes along the entire Garfield Avenue frontage of APN 155-301-02 per Public Works Standard Plan No. 102. (GP, ZSO 230.84)
 - c. A blanket easement over the RV Storage private streets and/or access ways for Police and Fire Department emergency access purposes.
5. A reproducible Mylar copy and a print of the recorded Final Parcel Map shall be submitted to the Department of Public Works at the time of recordation.
6. The engineer or surveyor preparing the Final Parcel Map shall comply with Sections 7-9-330 and 7-9-337 of the Orange County Subdivision Code and Orange County Subdivision Manual, Subarticle 18 for the following item:
 - d. Tie the boundary of the map into the Horizontal Control System established by the County Surveyor.
 - e. Provide a digital-graphics file of said map to the County of Orange.
7. Provide a digital-graphics file of said map to the City per the following design criteria:
 - f. Design Specification:
 - i. Digital data shall be full size (1:1) and in compliance with the California coordinate system – STATEPLANE Zone 6 (Lambert Conformal Conic projection), NAD 83 datum in accordance with the County of Orange Ordinance 3809.
 - ii. Digital data shall have double precision accuracy (up to fifteen significant digits).
 - iii. Digital data shall have units in US FEET.
 - iv. A separate drawing file shall be submitted for each individual sheet.
 - v. Digital data shall be in compliance with the Huntington Beach Standard Sheets, drawing names, pen color and layering conventions.
 - vi. Feature compilation shall include, but shall not be limited to: Assessor's Parcel Numbers (APN), street addresses and street names with suffix.
 - g. File Format and Media Specification:
 - i. Shall be in compliance with one of the following file formats (AutoCAD DWG format preferred):

- AutoCAD (version 2000, release 4) drawing file: __.DWG
 - Drawing Interchange file: __.DXF
- ii. Shall be in compliance with the following media type:
- CD Recordable (CD-R) 650 Megabytes
8. All applicable Public Works fees shall be paid. Fees shall be calculated based on the currently approved rate at the time of payment. (ZSO 250.16)

THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO ISSUANCE OF A DEMOLITION PERMIT:

9. Applicant shall provide a consulting arborist report on all the existing trees. Said report shall quantify, identify, size and analyze the health of the existing trees. The report shall also recommend how the existing trees that are to remain (if any) shall be protected and how far construction/grading shall be kept from the trunk. (Resolution 4545)
- a. Existing mature trees that are to be removed must be replaced at a 2 for 1 ratio with a 36" box tree or palm equivalent (13'-14' of trunk height for Queen Palms and 8'-9' of brown trunk).

THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO ISSUANCE OF A GRADING PERMIT:

10. The Final Parcel Map shall be recorded with the County of Orange.
11. The following dedications to the City of Huntington Beach shall be shown on the Precise Grading Plan. (ZSO 230.084A)
- a. A 20-foot right-of-way dedication for public street and public utilities purposes along the entire Ward Street frontage of APN 155-301-02 per Public Works Standard Plan No. 102. (GP, ZSO 230.84)
- b. A 20-foot right-of-way dedication for public street and public utilities purposes along the entire Garfield Avenue frontage of APN 155-301-02 per Public Works Standard Plan No. 102. (GP, ZSO 230.84)
12. An Improvement Plan prepared by a Licensed Civil Engineer, shall be submitted to the Department of Public Works for review and approval. (MC 17.05/ZSO 230.84) The plans shall comply with Public Works plan preparation guidelines and include the following improvements on the plan:
- a. New curb, gutter and sidewalk along the entire Ward Street frontage of APN 155-301-02 per City Standard Plan Nos. 202 and 207. (ZSO 255.04)
- b. New pavement for the easterly side of the Ward Street frontage (From existing edge of pavement to the new gutter edge) per City Standard Plan No. 103. (ZSO 255.04)
- c. The proposed driveway approaches on Ward Street shall be constructed per City Standard Plan No. 211. (ZSO 230.84)
- d. Street lights shall be installed along the entire Ward Street project frontage of APN 155-301-02. Street lighting plans shall be prepared by a Licensed Civil or

- Electrical Engineer in accordance with the City of Huntington Department of Public Works guidelines. (ZSO 230.84)
- e. A curb return shall be installed on the southeast corner of Garfield Avenue and Ward Street. Due to possible conflicts with the existing high voltage electrical tower at this corner, a modified curb return design shall be submitted to the Department of Public Works for review and approval. (ZSO 230.84)
 - f. An ADA compliant access ramp at the southeast corner of Garfield Avenue and Ward Street per Caltrans Standard Plan A88A. (ZSO 230.84, ADA)
 - g. Currently, there are no public sanitary sewer facilities on Ward Street in the project vicinity to serve the development. A public sanitary sewer main shall be designed and constructed to run southerly on Ward Street to Sunday Drive and turn westerly to connect to the manhole just to west of the intersection of Ward Street and Sunday Drive. (ZSO 255)
13. A Precise Grading Plan prepared by a Licensed Civil Engineer, shall be submitted to the Department of Public Works for review and approval. (MC 17.05/ZSO 230.84) The plans shall comply with Public Works plan preparation guidelines and include the following improvements on the plan:
 - a. A new domestic water service and meter shall be installed per Water Division Standards, and sized to meet the minimum requirements set by the California Plumbing Code (CPC) (MC 14.08.020)
 - b. A separate irrigation water service and meter shall be installed per Water Division Standards. (ZSO 232)
 - c. A separate backflow protection device shall be installed per Water Division Standards for domestic, irrigation, and fire water services. (Resolution 5921 and Title 17)
 - d. A new sewer lateral shall be installed and be sized to accommodate the proposed RV Storage office building restroom and dump/wash station. (ZSO 230.84)
 14. A Signing and Striping Plan for Ward Street shall be prepared by a Licensed Civil or Traffic Engineer and be submitted to the Department of Public Works for review and approval. The plans shall be prepared according to the City of Huntington Beach Signing and Striping Plan Preparation Guidelines. (ZSO 230.84)
 15. Based on the Fire Department's requirement for a private on-site fire hydrant system, a hydraulic water analysis is required to identify any off-site water improvements necessary to adequately protect the property per the Fire Department requirements. The developer shall be required to upgrade/improve the City's water system per City Water Standards to meet the water demands to the site and/or otherwise mitigate the impacts of the property at no cost to the City. The developer shall provide the City with a site plan showing the existing and proposed on-site and off-site water improvements (including pipeline sizes, fire hydrants, meters, and backflow device locations). The developer shall be responsible to pay the City for performing the water analysis using the City's hydraulic water model. (SMA 66428.1(d) and ZSO 255.04(E))
 16. The existing 36 inch steel reclaimed waterline that crosses the development portion of the site is an Orange County Water District (O.C.W.D.) Facility. This O.C.W.D. waterline and related facilities shall be shown on all the development plans for the project. The

construction of any improvements over the waterline and its related facilities shall be coordinated and approved by O.C.W.D. prior to the issuance of a Precise Grading Permit.

17. A Landscape and Irrigation Plan, prepared by a Licensed Landscape Architect shall be submitted to the Public Works Department for review and approval by the Public Works and Planning Departments. (ZSO 232.04)
 - a. Existing mature trees that are to be removed must be replaced at a 2 for 1 ratio with a 36" box tree or palm equivalent (13'-14' of trunk height for Queen Palms and 8'-9' of brown trunk).
 - b. "Smart irrigation controllers" and/or other innovative means to reduce the quantity of runoff shall be installed. (ZSO 232.04D)
 - c. Standard landscape code requirements apply. (ZSO 232)
18. All landscape planting, irrigation and maintenance shall comply with the City Arboricultural and Landscape Standards and Specifications. (ZSO 232.04B)
19. Landscaping plans should utilize native, drought-tolerant landscape materials where appropriate and feasible. (DAMP)
20. The Consulting Arborist (approved by the City Landscape Architect) shall review the final landscape tree planting plan and approve in writing the selection and locations proposed for new trees and the protection measures and locations of existing trees to remain. Said Arborist report shall be incorporated onto the Landscape Architect's plans as construction notes and/or construction requirements. The report shall include the Arborist's name, certificate number and the Arborist's wet signature on the final plan. (Resolution-4545)
21. There are no existing public storm drain facilities on Ward Street in the project vicinity. A hydrology and hydraulic analysis for all necessary on and off-site drainage improvements shall be submitted to the Department of Public Works for review and approval (10, 25, and 100-year storms and back to back storms shall be analyzed). In addition, if on-site detention is proposed, this study shall include 24-hour peak back-to-back 100-year storms for detention analysis and shall include ponding exhibits. The drainage improvements shall be designed and constructed as required by the Department of Public Works to mitigate impact of increased runoff due to development, or deficient, downstream systems. Design of all necessary drainage improvements shall provide mitigation for all rainfall event frequencies up to a 100-year frequency. (ZSO 230.84)
22. Prior to the issuance of any grading or building permits for projects that will result in soil disturbance of one or more acres of land, the applicant shall demonstrate that coverage has been obtained under California's General Permit for Stormwater Discharges Associated with Construction Activity by providing a copy of the Notice of Intent (NOI) submitted to the State Water Resources Control Board and a copy of the subsequent notification of the issuance of a Waste Discharge Identification (WDID) Number. Projects subject to this requirement shall prepare and implement a Stormwater Pollution Prevention Plan (SWPPP) conforming to the current National Pollution Discharge Elimination System (NPDES) requirements shall be submitted to the Department of Public Works for review and acceptance. A copy of the current SWPPP shall be kept at the project site and another copy to be submitted to the City. (DAMP)

23. A Project Water Quality Management Plan (WQMP) conforming to the City of Huntington Beach's Project WQMP Preparation Guidance Manual dated June 2006 and prepared by a Licensed Civil Engineer, shall be submitted to the Department of Public Works for review and acceptance and shall include the following:
- a. Discusses regional or watershed programs (if applicable).
 - b. Addresses Site Design BMPs (as applicable) such as minimizing impervious areas, maximizing permeability, minimizing directly connected impervious areas, creating reduced or "zero discharge" areas, and conserving natural areas.
 - c. Incorporates the applicable Routine Source Control BMPs as defined in the Drainage Area Management Plan. Source Control BMPs shall be implemented as appropriate to protect groundwater quality from target pollutants associated with this type of development such as antifreeze, oil and grease leaks from RVs and boats and any other pollutants identified by the applicant. (DAMP)
 - d. Incorporates Treatment Control BMPs as defined in the DAMP.
 - e. Generally describes the long-term operation and maintenance requirements for the Treatment Control BMPs.
 - f. Identifies the entity that will be responsible for long-term operation and maintenance of the Treatment Control BMPs.
 - g. Describes the mechanism for funding the long-term operation and maintenance of the Treatment Control BMPs.
 - h. Includes an Operations and Maintenance (O&M) Plan for all structural BMPs.
 - i. After incorporating plan check comments of Public Works, three final WQMPs (signed by the owner and the Registered Civil Engineer of record) shall be submitted to Public Works for acceptance. After acceptance, two copies of the final report shall be returned to applicant for the production of a single complete electronic copy of the accepted version of the WQMP on CD media that includes:
 - i) The 11" by 17" Site Plan in .TIFF format (400 by 400 dpi minimum).
 - ii) The remainder of the complete WQMP in .PDF format including the signed and stamped title sheet, owner's certification sheet, Inspection/Maintenance Responsibility sheet, appendices, attachments and all educational material.
 - j. The applicant shall return one CD media to Public Works for the project record file.
24. Indicate the type and location of Water Quality Treatment Control Best Management Practices (BMPs) on the Grading Plan consistent with the Project WQMP. The WQMP shall follow the City of Huntington Beach; Project Water Quality Management Plan Preparation Guidance Manual dated June 2006. The WQMP shall be submitted with the first submittal of the Grading Plan.
25. In order to protect groundwater resources and to avoid clogging from excessive siltation and debris, any structural infiltration BMP shall meet the following requirements:
- a. The vertical distance from the bottom of the infiltration system to the seasonal high groundwater shall be at least 10 feet.

- b. The infiltration system shall be located at least 100 feet horizontally from any water supply wells.
 - c. The infiltration BMP shall be preceded by an acceptable pretreatment BMP device such as grit chambers, swales with check dams, filter strips or sediment forebays/traps. Such pretreatment device shall be appropriately designed to remove target pollutants associated with this type of development prior to discharge to the infiltration BMP.
26. The proposed on-site designated vehicle wash area shall not drain to the storm drain system. Wash water from this area shall be directed to the sanitary sewer (upon approval by the Orange County Sanitation District), to an engineered infiltration system, or to an equally effective alternative. Pre-treatment may also be required. (DAMP)
27. The on-site sewer dump station shall be constructed to have a minimum of forty-eight inch (48") by forty-eight inch (48") concrete floor graded and sloping downward to the sewer riser equipped with a cap to prevent stormwater from entering the system. The sewer disposal point shall have a concrete containment wall installed around the disposal point, no less than six inches (6") in height. A standard RV/boat sewer dump instruction sign approved by the Department of Public Works shall be posted at the station.
28. A suitable location, as approved by the City, shall be depicted on the grading plan for the necessary trash enclosure. The area shall be paved with an impervious surface, designed not to allow run-on from adjoining areas, designed to divert drainage from adjoining roofs and pavements diverted around the area, and screened or walled to prevent off-site transport of trash. The trash enclosure area shall be covered or roofed with a solid, impervious material. Connection of trash area drains into the storm drain system is prohibited. The trash enclosure area shall be connected to the sanitary sewer. (DAMP)
29. A detailed soils and geological analysis shall be prepared by a registered engineer. This analysis shall include on-site soil sampling and laboratory testing of materials to provide detailed recommendations for grading, over excavation, engineered fill, dewatering, settlement, protection of adjacent structures, chemical and fill properties, liquefaction, retaining walls, streets, and utilities and infiltration rates for existing onsite soils. The soils report shall specifically address the required pavement sections necessary to accommodate long-term parking of heavy vehicles such as RVs and boats. (MC 17.05.150)
30. The applicant's grading/erosion control plan shall abide by the provisions of AQMD's Rule 403 as related to fugitive dust control. (AQMD Rule 403)
31. The name and phone number of an on-site field supervisor hired by the developer shall be submitted to the Planning and Public Works Departments. In addition, clearly visible signs shall be posted on the perimeter of the site every 250 feet indicating who shall be contacted for information regarding this development and any construction/grading-related concerns. This contact person shall be available immediately to address any concerns or issues raised by adjacent property owners during the construction activity. He/She will be responsible for ensuring compliance with the conditions herein, specifically, grading activities, truck routes, construction hours, noise, etc. Signs shall include the applicant's contact number, regarding grading and construction activities, and "1-800-CUTSMOG" in the event there are concerns regarding fugitive dust and compliance with AQMD Rule No. 403.

32. The applicant shall notify all property owners and tenants within 300 feet of the perimeter of the property of a tentative grading schedule at least 30 days prior to such grading.

THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLIED WITH DURING GRADING OPERATIONS:

33. An Encroachment Permit is required for all work within the City's right-of-way. (MC 12.38.010/MC 14.36.030)
34. The developer shall coordinate the development of a truck haul route with the Department of Public Works if the import or export of material in excess of 5000 cubic yards is required. This plan shall include the approximate number of truck trips and the proposed truck haul routes. It shall specify the hours in which transport activities can occur and methods to mitigate construction-related impacts to adjacent residents. These plans must be submitted for approval to the Department of Public Works. (MC 17.05.210)
35. Water trucks will be utilized on the site and shall be available to be used throughout the day during site grading to keep the soil damp enough to prevent dust being raised by the operations. (California Stormwater BMP Handbook, Construction Wind Erosion WE-1)
36. All haul trucks shall arrive at the site no earlier than 8:00 a.m. or leave the site no later than 5:00 p.m., and shall be limited to Monday through Friday only. (MC 17.05)
37. Wet down the areas that are to be graded or that is being graded, in the late morning and after work is completed for the day. (WE-1/MC 17.05)
38. The construction disturbance area shall be kept as small as possible. (California Stormwater BMP Handbook, Construction Erosion Control EC-1) (DAMP)
39. All haul trucks shall be covered or have water applied to the exposed surface prior to leaving the site to prevent dust from impacting the surrounding areas. (DAMP)
40. Prior to leaving the site, all haul trucks shall be washed off on-site on a gravel surface to prevent dirt and dust from leaving the site and impacting public streets. (DAMP)
41. Comply with appropriate sections of AQMD Rule 403, particularly to minimize fugitive dust and noise to surrounding areas. (AQMD Rule 403)
42. Wind barriers shall be installed along the perimeter of the site. (DAMP)
43. All construction materials, wastes, grading or demolition debris and stockpiles of soils, aggregates, soil amendments, etc. shall be properly covered, stored and secured to prevent transport into surface or ground waters by wind, rain, tracking, tidal erosion or dispersion. (DAMP)

THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO ISSUANCE OF A BUILDING PERMIT:

44. A Precise Grading Permit shall be issued. (MC 17.05)
45. Traffic impact fees for the commercial development shall be paid at the applicable rate at the time and prior to Building Permit issuance. The current rate is \$163 per net new added daily trip and is adjusted annually on December 1. The new added daily trips shall be calculated by City staff or determined from applicant provided traffic survey data of a similar site. (MC 17.65)

**THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO
ISSUANCE OF AN ENCROACHMENT PERMIT:**

46. Traffic Control Plans, prepared by a Licensed Civil or Traffic Engineer, shall be prepared in accordance with the latest edition of the City of Huntington Beach Construction Traffic Control Plan Preparation Guidelines and submitted to the Department of Public Works for review and approval. (Construction Traffic Control Plan Preparation Guidelines)

**THE FOLLOWING DEVELOPMENT REQUIREMENTS SHALL BE COMPLETED PRIOR TO
FINAL INSPECTION OR OCCUPANCY:**

47. Complete all improvements as shown on the approved grading, improvement, and landscape plans. (MC 17.05)
48. All new utilities shall be undergrounded. (MC 17.64)
49. All applicable Public Works fees shall be paid at the current rate unless otherwise stated, per the Public Works Fee Schedule adopted by the City Council and available on the city web site at http://www.surfcity-hb.org/files/users/public_works/fee_schedule.pdf. (ZSO 240.06/ZSO 250.16)
50. The current tree code requirements shall apply to this site. (ZSO 232)
 - a. Existing trees to remain on site shall not be disfigured or mutilated, (ZSO 232.04E)
 - b. General tree requirements, regarding quantities and sizes. (ZSO 232.08B and C)
51. All landscape irrigation and planting installation shall be certified to be in conformance to the City approved landscape plans by the Landscape Architect of record in written form to the City Landscape Architect. (ZSO 232.04D)
52. Applicant shall provide City with CD media TIFF images (in City format) and CD (AutoCAD only) copy of complete City Approved landscape construction drawings as stamped "Permanent File Copy" prior to starting landscape work. Copies shall be given to the City Landscape Architect for permanent City record.
53. The Water Ordinance #14.52, the "Water Efficient Landscape Requirements" apply for projects with 2500 square feet of landscaping and larger. (MC 14.52)
54. Prior to grading or building permit close-out and/or the issuance of a certificate of use or a certificate of occupancy, the applicant shall:
 - a. Demonstrate that all structural Best Management Practices (BMPs) described in the Project WQMP have been constructed and installed in conformance with approved plans and specifications.
 - b. Demonstrate all drainage courses, pipes, gutters, basins, etc. are clean and properly constructed.
 - c. Demonstrate that applicant is prepared to implement all non-structural BMPs described in the Project WQMP.
 - d. Demonstrate that an adequate number of copies of the approved Project WQMP are available for the future occupiers.



HUNTINGTON BEACH FIRE DEPARTMENT

PROJECT IMPLEMENTATION CODE REQUIREMENTS

DATE: SEPTEMBER 10, 2009

PROJECT NAME: WARD RV STORAGE

ENTITLEMENTS: CONDITIONAL USE PERMIT NO. 09-024

PROJECT LOCATION: 19118 WARD STREET, HUNTINGTON BEACH, CA

PLANNER: JANE JAMES, ASSOCIATE PLANNER

TELEPHONE/E-MAIL: (714) 536-5596/ jjames@surfcity-hb.org

PLAN REVIEWER-FIRE: DARIN MARESH, FIRE DEVELOPMENT SPECIALIST

TELEPHONE/E-MAIL: (714) 536-5531/ dmaresh@surfcity-hb.org

PROJECT DESCRIPTION: TO PERMIT THE ESTABLISHMENT OF A NEW 557 SPACE RECREATIONAL STORAGE FACILITY ON A 13.52 ACRES IN I NCONJUNCTION WITH THE ADOPTION OF THE WARD GARFIELD SPECIFIC PLAN NO 14 FOR A 43.60 ACRE PORTION OWNED BY SOUTHER CALIFORNIA EDISON. THE GPA IS TO ESTABLISH THE SPECIFIC PLAN OVERLAY; THE ZMA IS TO AMEND THE ZONING FROM RL TO SPECIFIC PLAN NO. 14, THE ZTA IS TO ADOPT SPECIFIC PLAN NO. 14 AND THE CUP IS TO ESTABLISH TH RV STORGAE FACILITY.

The following is a list of code requirements deemed applicable to the proposed project based on plans received and dated August 20, 2008. The list is intended to assist the applicant by identifying requirements which must be satisfied during the various stages of project permitting and implementation. A list of conditions of approval adopted by the Planning Commission in conjunction with the requested entitlement(s), if any, will also be provided upon final project approval. If you have any questions regarding these requirements, please contact the Plan Reviewer- Fire: DARIN MARESH, FIRE DEVELOPMENT SPECIALIST.

PRIOR TO DEMOLITION, GRADING, SITE DEVELOPMENT, ISSUANCE OF GRADING PERMITS, BUILDING PERMITS, AND/OR CONSTRUCTION, THE FOLLOWING SHALL BE REQUIRED:

Fire Apparatus Access

Fire Access Roads shall be provided and maintained in compliance with City Specification # 401, *Minimum Standards for Fire Apparatus Access*. Driving area shall be capable of supporting a fire apparatus (75,000 lbs and 12,000 lb point load). Minimum fire access road width is twenty-four feet (24') wide, with thirteen feet six inches (13' 6") vertical clearance. Fire access roads

ATTACHMENT NO. 6.10

fronting commercial buildings shall be a minimum width of twenty-six feet (26') wide, with thirteen feet six inches (13' 6") vertical clearance. For Fire Department approval, reference and demonstrate compliance with City Specification # 401 *Minimum Standards for Fire Apparatus Access* on the plans. **(FD)**

Fire Access Road Turns and Corners shall be designed with a minimum inner radius of seventeen feet (17') and a minimum outer radius of forty five feet (45') per City Specification # 401 *Minimum Standards for Fire Apparatus Access*. For Fire Department approval, reference and demonstrate compliance with City Specification # 401 *Minimum Standards for Fire Apparatus Access* on the plans. **(FD)**

Reciprocal Access. Existing designated 24 foot wide fire apparatus access roads (shared as a 12'/12' reciprocal fire apparatus access road/driveway shall be preserved and maintained in compliance with City Specification # 401, *Minimum Standards for Fire Apparatus Access*. Overhead clearance of 13' 6" shall be maintained and obstructions such as roof eaves shall not project into the designated fire lane minimum overhead clearance. **(FD)**

Fire Lanes, as determined by the Fire Department, shall be posted, marked, and maintained per City Specification #415, *Fire Lanes Signage and Markings on Private, Residential, Commercial and Industrial Properties*. The site plan shall clearly identify all red fire lane curbs, both in location and length of run. The location of fire lane signs shall be depicted. No parking shall be allowed in the designated 24 foot wide fire apparatus access road or supplemental fire access per City Specification # 415. For Fire Department approval, reference and demonstrate compliance with City Specification # 401 *Minimum Standards for Fire Apparatus Access* on the plans. **(FD)**

Secured Vehicle Entries shall utilize KNOX[®] activated access switches (Knox switches for automated gates, Knox padlocks for manual gates), and comply with City Specification #403, *Fire Access for Pedestrian or Vehicular Security Gates & Buildings*. Reference compliance in the plan notes. **(FD)**

Secured Automated Vehicle Entry Gates (Residential) shall utilize a combination "Strobe-Activated Switch" and "Knox Manual Key Switch", and comply with *City Specification # 403, Fire Access for Pedestrian or Vehicular Security Gates & Buildings*. Reference compliance with *City Specification # 403 Fire Access for Pedestrian or Vehicular Security Gates & Buildings* in the plan notes. **(FD)**

Fire Hydrants and Water Systems

Private Fire Hydrants are required. Hydrants must be portrayed on the site plan. Hydrants shall be installed and in service **before** combustible construction begins. Installation of hydrants and service mains shall meet NFPA 13 and 24, 2002 Edition, Huntington Beach Fire Code Appendix B and C, and City Specification # 407 Fire Hydrant Installation Standards requirements. Private

fire hydrants shall not be pressurized by Fire Department Connections to the sprinkler system. The system design shall ensure that recirculation of pressurized water from the hydrant, thru the FDC and back through the sprinkler system supply to the hydrant does not occur. Installation of the private fire service main, including fire department connections, shall meet NFPA 13 and 24, 2002 Edition requirements. Maximum allowed velocity of fire flow in supply piping is 12 fps. The maintenance of private fire hydrants is the responsibility of the owner or facility association. Shop drawings shall be submitted to and approved by the Fire Department. For Fire Department approval, portray the fire hydrants and reference compliance with City Specification #407 Fire Hydrant Installation Standards in the plan notes. **(FD)**

Private Fire Service Piping (FSP) Application for permit shall be made for private on-site Fire Service Piping (FSP), including but not limited to, private fire service mains and underground sprinkler laterals. Maximum allowed velocity of fire flow in supply piping is 12 fps. Additionally, application for permit shall be made for fire protections systems (sprinklers, alarms, chemical, fire pumps, etc.) as applicable.

Permits may be obtained at the City of Huntington Beach Department Fire Department by completing a Fire Permit Form (available at Fire Administration) and submitting such plans and specifications as required by the bureau of fire prevention. A permit constitutes permission to begin work in accordance with approved plans and specifications. The permit fee includes plan checking and inspections by an authorized fire prevention inspector. Development reviews/approvals by the bureau of fire prevention during planning do not constitute approval to perform FSP or fire protection system work, unless otherwise noted. **(FD)**

Private Fire Service Connection to the Public Water Supply - Separate plans shall be submitted to the Public Works Department detailing the connection, piping, valves and back-flow prevention assembly (DDCA) for approval and permits. Approval by Public Works and the Fire Department must be completed prior to issuance of a grading permit. The dedicated private fire water service off-site improvements shall be shown on a precise grading plan, prepared by a Licensed Civil Engineer. **(FD)**

Fire Protection Systems

Fire Extinguishers shall be installed and located in all areas to comply with Huntington Beach Fire Code standards found in *City Specification #424*. The minimum required dry chemical fire extinguisher size is 2A 10BC and shall be installed within 75 feet travel distance to all portions of the building. Extinguishers are required to be serviced or replaced annually. **(FD)**

GIS Mapping Information

- a. **GIS Mapping Information** shall be provided to the Fire Department in compliance with GIS Department CAD Submittal Guideline requirements. Minimum submittals shall include the following:

- Site plot plan showing the building footprint.
- Specify the type of use for the building
- Location of electrical, gas, water, sprinkler system shut-offs.
- Fire Sprinkler Connections (FDC) if any.
- Knox Access locations for doors, gates, and vehicle access.
- Street name and address.

Final site plot plan shall be submitted in the following digital format and shall include the following:

- Submittal media shall be via CD rom to the Fire Department.
- Shall be in accordance with County of Orange Ordinance 3809.
- File format shall be in .shp, AutoCAD, AUTOCAD MAP (latest possible release) drawing file - .DWG (preferred) or Drawing Interchange File - .DXF.
- Data should be in NAD83 State Plane, Zone 6, Feet Lambert Conformal Conic Projection.
- Separate drawing file for each individual sheet.
In compliance with Huntington Beach Standard Sheets, drawing names, pen colors, and layering convention. and conform to *City of Huntington Beach Specification # 409 – Street Naming and Addressing*.

For specific GIS technical requirements, contact the Huntington Beach GIS Department at (714) 536-5574.

For Fire Department approval, reference compliance with *GIS Mapping Information* in the building plan notes. **(FD)**

THE FOLLOWING CONDITIONS SHALL BE MAINTAINED DURING CONSTRUCTION:

- a. Fire/Emergency Access And Site Safety shall be maintained during project construction phases in compliance with HBFC Chapter 14, Fire Safety During Construction And Demolition. **(FD)**
- b. Fire/Emergency Access And Site Safety shall be maintained during project construction phases in compliance with City Specification #426, Fire Safety Requirements for Construction Sites. **(FD)**

Fire Department City Specifications may be obtained at:
Huntington Beach Fire Department Administrative Office
City Hall 2000 Main Street, 5th floor
Huntington Beach, CA 92648
or through the City's website at **www.surfcity-hb.org**

If you have any questions, please contact the Fire Prevention Division at (714) 536-5411.



**CITY OF HUNTINGTON BEACH
DEPARTMENT OF BUILDING & SAFETY
PROJECT IMPLEMENTATION CODE REQUIREMENTS**

DATE: SEPTEMBER 10, 2009
PROJECT NAME: RV PARKING
ENTITLEMENTS: PLANNING APPLICATION NO. 2009-197; GPA NO. 09-002; ZMA NO. 09-002; ZTA NO. 09-007; CUP NO. 09-024; EA NO. 09-006
PROJECT LOCATION: 19118 WARD STREET
PROJECT PLANNER: JANE JAMES, SENIOR PLANNER
DATE OF PLANS: AUGUST 14, 2009
PLAN REVIEWER: JASON KWAK, PLAN CHECK ENGINEER
TELEPHONE/E-MAIL: (714) 536-5278 / jkwak@surfcity-hb.org
PROJECT DESCRIPTION: To permit the establishment of a new 557 space recreation vehicle storage facility on an approximately 13.52 acre property in conjunction with the adoption of the Ward Garfield Specific Plan for a 43.6 acre portion owned by Southern California Edison.

The following is a list of code requirements deemed applicable to the proposed project based on plans received as stated above. The list is intended to assist the applicant by identifying requirements which must be satisfied during the various stages of project permitting and implementation. This list is not intended to be a full and complete list and serves only to highlight possible building code issues on the proposed preliminary plans. Electrical, plumbing, and mechanical items are not included in this review. If you have any questions regarding these comments, please contact the plan reviewer.

I. SPECIAL CONDITIONS:

1. None

II. CODE ISSUES BASED ON PLANS & DRAWINGS SUBMITTED:

1. Project shall comply with the current state building codes adopted by the City at the time of permit application submittal. Currently they are 2007 California Building Code (CBC), 2007 California Mechanical Code, 2007 California Plumbing Code, 2007 California Electrical Code, 2007 California Energy Code and the Huntington Beach Municipal Code (HBMC). Compliance to all applicable state and local codes is required prior to issuance of building permit.

ATTACHMENT NO. 6.14

**ENVIRONMENTAL CHECKLIST FORM
CITY OF HUNTINGTON BEACH
PLANNING DEPARTMENT
ENVIRONMENTAL ASSESSMENT NO. 09-006**

- 1. PROJECT TITLE:** **WARD GARFIELD SPECIFIC PLAN
(Huntington Beach RV Storage)**
- Concurrent Entitlements:** General Plan Amendment No. 09-002
Zoning Map Amendment No. 09-002
Zoning Text Amendment No. 09-007
Conditional Use Permit No. 09-024
- 2. LEAD AGENCY:** City of Huntington Beach
2000 Main Street
Huntington Beach, CA 92648
- Contact:** Jane James, Senior Planner
Phone: (714) 536-5271
- 3. PROJECT LOCATION:** Southeast corner of Garfield Avenue and Ward Street
(generally bounded by Ward Street, Garfield Avenue, Santa Ana River Channel, Arevalos Park and Mariner's Pointe Mobilehome Park)
- 4. PROJECT PROPONENT:** Huntington RV Storage, LLC
Contact Person: Doc Rivers
Phone: (805) 501-3508
- 5. GENERAL PLAN DESIGNATION:** Existing: P (Public)
Proposed: P-sp (Public – Specific Plan)
- 6. ZONING:** Existing: RL (Residential Low Density)
Proposed: Ward Garfield Specific Plan No. 16
- 7. PROJECT DESCRIPTION:**

Proposed Project

The Ward Garfield Specific Plan No. 16 development concept provides for a recreational vehicle storage facility within a portion of the SCE right-of-way that parallels the Santa Ana River Channel on the eastern boundary of the City. The development concept also recognizes the existing Village Nurseries, electrical substation and utilities, and wireless communication facilities.

The applicant requests a General Plan Amendment, Zoning Map Amendment, Zoning Text Amendment, and Conditional Use Permit to adopt a new specific plan allowing for the establishment of an Recreational Vehicle (RV) Storage facility on approximately 13.52 acres at the south end of the project site. The proposed project will also require submittal of a tentative and final parcel map to establish lease lines at the subject property. It is not necessary to process the subdivision at this time, but it will be required prior to issuance of a grading permit. Specifically, the applications proposed at this time are as follows:

General Plan Amendment No 09-002: To add a Specific Plan suffix (“sp”) to the existing Public land use designation

Zoning Map Amendment No. 09-002: To amend the zoning map from the current Residential Low Density designation to the Ward Garfield Specific Plan No. 16 designation.

Zoning Text Amendment No. 09-007: To establish the Ward Garfield Specific Plan No. 16 document.

Conditional Use Permit No. 09-024: To permit recreational vehicle storage on 13.52 acres of the total 43.60 acre specific plan area.

The proposed RV storage lot will consist of 557± RV parking spaces, a 480 sq. ft. modular rental office with restroom, and associated perimeter fencing and lighting. There will be a dump station for grey and black water, a vehicle washing area, and a trash enclosure inside the facility. Existing transmission towers and electrical lines will continue to exist throughout the RV parking facility. The recreational vehicles may include campers, motorhomes, boats, trailers, toy haulers, jet skis, and similar type vehicles and towing apparatus. Approximately six different parking stalls sizes will be provided throughout the site ranging in size from 10 ft. by 20 ft. to 11 ft. by 40 ft. No on-site living in the vehicles will be permitted and no vehicular repair will be allowed on-site. Approximately one person will be employed during regular business hours while a second person will perform security duties at other hours.

The new RV Storage facility is designed to provide access from Ward Street with a large off-street queuing area to an automated remote gate system. The remote gate system will be designed to allow tenants to activate the gate opening at a substantial distance away, while still traveling on Ward Street, so that it is open when they actually arrive. Furthermore, the gate will be setback approximately 120 feet from the street so that at least three large motorhomes would be able to queue without impacting through traffic on Ward Street. Should the gate be closed and unopenable, vehicles are also provided with a sufficient turn around space to reenter the public street system without the need for backing up.

The proposed hours of operation for the RV storage office use are as follows:

Office Hours:

Sunday through Friday from 8:00 AM to 5:00 PM

Saturday from 7:00 AM to 6:00 PM

Remote Gate Access Hours:

Daily from 5:00 AM to 7:00 PM

Should a tenant desire to enter the site outside of the remote gate access hours, special arrangements may be made with the office personnel to accommodate those needs.

Approximately 8.8% of the site would be landscaped with the majority of the landscaping installed in more visible areas at the front entry and within the proposed ornamental fencing abutting existing residential properties. Construction is expected to take approximately two months.

The proposed Ward Garfield Specific Plan No. 16 proposes to designate three separate planning areas for the entire 43.60 acre site as follows:

Planning Area 1 (13.52 Acres): Huntington Beach RV Storage Facility

Planning Area 2 (12.95 Acres): Southern California Substation

Planning Area 3 (17.13 Acres): Village Nurseries

No changes to the hours of operation or the facility operations for Planning Area 2 or Planning Area 3 are proposed, other than to consolidate the current Village Nursery operations from Planning Area 1 to Planning Area 3.

Existing Conditions

The project site consists of approximately 43.60 acres at the southeast corner of Ward Street and Garfield Avenue at the east end of the city. The site is owned and operated by Southern California Edison (SCE) who currently leases portions of the property to landscape nursery uses. A subdivision will be required to establish lease lines for the current and proposed uses. The subject property is located on one parcel as well as a portion of a second parcel.

The approximately 13 acre Ellis Substation consists of transmission equipment and power distribution lines along with the substation office building. The onsite operations are power distribution services with approximately eight to ten employees. The facility is manned between 7:00AM and 5:00PM daily. The office building is approximately 2,000 square feet in size. A separate service truck parking building is also located on site. Access to the SCE substation is provided from Ward Street through the area leased to the nursery operation. No changes to the substation are proposed.

Village Nurseries Landscape Center has been in operation at the site since the 1970s (previously known as Nina's Nursery). The nursery has operated through 5-year renewable lease agreements with SCE and consists of wholesale of in ground and potted plants and trees. The business operates out of an open wood structure adjacent to two modular buildings joined to serve as an office. There are ten employees and laborers employed at the nursery.

The wholesale nursery is open daily to the public with hours of operation generally between 7:00AM and 5:00PM. Customer access is provided from Garfield Avenue with business related access from Ward Street. On-site parking is currently provided along Garfield Avenue for approximately 20 cars. In addition, equipment stored at the site consists of cart movers, small trucks, and delivery equipment.

Village Nurseries will consolidate their operation into the area surrounding the SCE substation in order to accommodate the new recreational vehicle storage operation. No additional facilities are proposed for the nursery operation.

8. **SURROUNDING LAND USES AND SETTING:** The site is surrounded by the Orange County Sanitation District uses to the north across Garfield Avenue (City of Fountain Valley); the Santa Ana River Channel to the east; Arevalos Park and single family residences to the south; Mariner's Pointe Mobilehome Park to the southwest; and single family residences to the west across Ward Street.
9. **OTHER PREVIOUS RELATED ENVIRONMENTAL DOCUMENTATION:** None

10. OTHER AGENCIES WHO'S APPROVAL IS REQUIRED (AND PERMITS NEEDED) (i.e. permits, financing approval, or participating agreement): California Public Utilities Commission

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" or is "Potentially Significant Unless Mitigated," as indicated by the checklist on the following pages.

- | | | |
|--|---|--|
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Transportation / Traffic | <input type="checkbox"/> Public Services |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Utilities / Service Systems |
| <input type="checkbox"/> Geology / Soils | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Aesthetics |
| <input type="checkbox"/> Hydrology / Water Quality | <input type="checkbox"/> Hazards and Hazardous Materials | <input type="checkbox"/> Cultural Resources |
| <input type="checkbox"/> Air Quality | <input type="checkbox"/> Noise | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Agriculture Resources | <input type="checkbox"/> Mandatory Findings of Significance | |

DETERMINATION

On the basis of this initial evaluation:

I find that the proposed project **COULD NOT** have a significant effect on the environment, and a **NEGATIVE DECLARATION** will be prepared.

I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on an attached sheet have been added to the project. A **MITIGATED NEGATIVE DECLARATION** will be prepared.

I find that the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.

I find that the proposed project **MAY** have a "potentially significant impact" or a "potentially significant unless mitigated impact" on the environment, but at least one impact (1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An **ENVIRONMENTAL IMPACT REPORT** is required, but it must analyze only the effects that remain to be addressed.

I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or **NEGATIVE DECLARATION** pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or **NEGATIVE DECLARATION**, including revisions or mitigation measures that are imposed upon the proposed project, **nothing further is required**.

Jane M. James
Signature

12.16.2009
Date

Jane James
Printed Name

Senior Planner
Title

EVALUATION OF ENVIRONMENTAL IMPACTS:

1. A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to the project. A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards.
2. All answers must take account of the whole action involved. Answers should address off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. “Potentially Significant Impact” is appropriate, if an effect is significant or potentially significant, or if the lead agency lacks information to make a finding of insignificance. If there are one or more “Potentially Significant Impact” entries when the determination is made, preparation of an Environmental Impact Report is warranted.
4. “Potentially Significant Impact Unless Mitigated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from Section XVIII, “Earlier Analyses,” may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). Earlier analyses are discussed in Section XVIII at the end of the checklist.
6. References to information sources for potential impacts (e.g., general plans, zoning ordinances) have been incorporated into the checklist. A source list has been provided in Section XVIII. Other sources used or individuals contacted have been cited in the respective discussions.
7. The following checklist has been formatted after Appendix G of Chapter 3, Title 14, California Code of Regulations, but has been augmented to reflect the City of Huntington Beach’s requirements.

(Note: Standard Conditions of Approval and Code Requirements - The City imposes standard conditions of approval and code requirements on projects which are considered to be components of or modifications to the project, some of these standard conditions also result in reducing or minimizing environmental impacts to a level of insignificance. However, because they are considered part of the project, they have not been identified as mitigation measures. For the readers’ information, a list of applicable standard conditions and code requirements identified in the discussions has been provided as Attachment No. 4.)

SAMPLE QUESTION:

ISSUES (and Supporting Information Sources):

<i>Potentially Significant Impact</i>	<i>Potentially Significant Unless Mitigation Incorporated</i>	<i>Less Than Significant Impact</i>	<i>No Impact</i>
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Would the proposal result in or expose people to potential impacts involving:

Landslides? (Sources: 1, 6)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: The attached source list explains that 1 is the Huntington Beach General Plan and 6 is a topographical map of the area which show that the area is located in a flat area. (Note: This response probably would not require further explanation).

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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I. LAND USE AND PLANNING. Would the project:

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? (Sources: 1, 2, 5, 23) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: The subject property is currently designated as Public (P) in the City’s General Plan and RL (Residential Low Density) in the Huntington Beach Zoning & Subdivision Ordinance (HBZSO). The existing land uses include an electrical substation and a landscape nursery business under the existing electrical transmission lines. Southern California Edison, a public utility company and the landowner of the subject site, has authorized submittal of the application to the City of Huntington Beach. The applicant is simultaneously applying to SCE for approval of the proposed improvements for RV storage. Therefore, the proposed project is not anticipated to conflict with any policies or regulations of SCE.

The proposed RV Storage lot will require a General Plan Amendment to add a specific plan suffix and Zoning Map and Zoning Text Amendments to establish a specific plan to allow the additional use. The project proponent wishes to establish recreational vehicle storage under the existing overhead power lines. Southern California Edison (SEC) does not permit construction of permanent improvements beneath the power lines but storage of movable vehicles would be allowed so SCE access to the electrical can always be maintained. According to General Plan Table LU-3, the specific plan overlay, “Permits underlying land uses and requires that a Specific or Development Plan be formulated for large scale, mixed-use multi-phased development projects which provides greater specificity for land use and infrastructure plans, design, and development standards, and phasing/implementation.”

Maintaining the current Public General Plan Land Use designation and adding the specific plan suffix is consistent with the following General Plan goals objectives, and policies:

Land Use Element

Objective LU13.1: Provide for the continuation of existing and development of new uses, such as governmental administrative, public safety, human service, cultural, educational, infrastructure, religious, and other uses that support the needs of existing and future residents and businesses.

Utilities Element

Objective U5.1: Ensure that adequate natural gas, telecommunication, and electrical systems are provided.

Policy U5.1.4: Require the review of new and or expansions of existing industrial and utility facilities to ensure that such facilities will not visually impair the City’s coastal corridors and entry nodes.

The proposed project adds a vehicle storage use beneath the existing overhead power lines without disrupting SCE’s ability to provide service to its customers. Additionally the proposed use will not visually impair coastal areas or entry nodes as the site is located on the eastern edge of the City, away from visually sensitive areas.

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	Potentially Significant No Impact
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The HBZSO classifies recreational vehicle storage under the Commercial land use category as follows: Vehicle Storage. The business of storing or safekeeping of operative and inoperative vehicles for periods of time greater than a 24 hour period, including, but not limited to, the storage of parking tow-aways, impound yards, and storage lots for automobiles, trucks, buses and recreational vehicles, but not including vehicle dismantling.

The HBZSO allows Vehicle Storage uses in both General Commercial and Industrial zoning categories. The specific plan is proposed to allow very specific uses on the property and to exclude other typical commercial and industrial uses because they would be not be appropriate adjacent to the residential and park uses nearby. The specific plan limits the allowable land uses to electrical substation and utilities, landscape nursery, RV storage, and wireless communication facilities. The other uses described above in the Vehicle Storage category, such as storage of parking tow-aways and impound yards, would not be permitted.

Development of the property under the existing Residential Low Density zoning is not likely as SCE has not indicated any proposal to remove or relocate the existing electrical utilities in the near future. Additionally, due to the unique narrow and long shape of the property and limited access to the public street system, residential development would be difficult to achieve. Thus, development of the property to the current RL zoning is not reasonably foreseeable.

The project site is not subject to provisions of the City's Local Coastal Program as the property is not located within the coastal zone boundaries. Less than significant impacts are anticipated.

- b) Conflict with any applicable habitat conservation plan or natural community conservation plan? (Sources: 1, 2)

Discussion: The project is proposed in an urbanized area and will coincide with existing electrical utilities. The project will not conflict with any habitat conservation plan or natural community conservation plan of the City of Huntington Beach, as there are no such plans adopted for the area.

- c) Physically divide an established community? (Sources: 1, 4, 5)

Discussion: The proposed development will occur on an existing parcel with direct access to an existing public street. No public access ways through the subject property exist. No new roadways, rail lines, bridges or other off-site improvements with the potential to physically divide an established community are proposed or required for the specific plan. The project does require dedication and improvement along the east side of Ward Avenue between the site entrance and Garfield Avenue. An existing dirt right-of-way will be improved with sidewalk, curb, gutter, and bike lane. A second travel lane and a striped median will be added, however the roadway improvements will not physically divide an established community. Less than significant impacts are anticipated.

II. POPULATION AND HOUSING. Would the project:

- a) Induce substantial population growth in an area, either directly (e.g., by proposing new homes and businesses) or indirectly (e.g., through extensions of roads or other infrastructure)? (Sources: 1, 5, 15, 23)

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Discussion: The proposed project will result in the establishment of a new RV storage facility that will not stimulate population growth in the area. The new business will provide storage facilities for existing RV owners in the area and no impact to population growth is anticipated.

- b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere? (Sources: 5, 23)
- c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere? (Sources: 5, 23)

Discussion: b) – c) The project site is currently occupied by the Ellis electrical substation and Village Nurseries Landscape Center. The proposed project provides for the establishment of a new RV Storage facility on a portion of the nursery. No residential uses or structures exist on the project site. Therefore, the proposed project will not displace existing housing or inhabitants and no impacts are anticipated.

III. GEOLOGY AND SOILS. Would the project:

- a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:
 - i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? (Sources: 1, 14, 17)

Discussion: According to the Limited Geotechnical Report by TGR Geotechnical, Inc., received and dated August 24, 2009, the project site is not known to be traversed by an active fault and is not located within the Alquist-Priolo Earthquake Fault Zone. The nearest faults are the San Joaquin Hills Blind Thrust Fault located 0.5 miles away and the Newport-Inglewood fault located approximately 2.5 miles southwest of the project site. No impacts are anticipated.

- ii) Strong seismic ground shaking? (Sources: 1, 14, 17)

Discussion: The project site is located in a seismically active region of Southern California. Therefore, the site could be subjected to strong ground shaking in the event of an earthquake. Structures built in Huntington Beach are required to comply with standards set forth in the California Building Code (CBC) and standard City codes, policies and procedures which require submittal of a detailed soils analysis prepared by a Licensed Soils Engineer. The required soils analysis must include on-site soil sampling and laboratory testing of materials to provide detailed recommendations regarding grading, foundations, retaining walls, streets, utilities, and chemical and fill properties of underground items including buried pipe and concrete and the protection thereof; and a report prepared by an engineering geologist indicating the ground surface acceleration from earth movement for the subject property. All structures shall be constructed in compliance with the g-factors as indicated by the geologist's report. Calculations for footings and structural members to

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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withstand anticipated g-factors must be submitted to the City for review prior to the issuance of building permits. Conformance with CBC requirements and standard City code requirements will ensure potential impacts from seismic ground shaking are reduced to a less than significant level.

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| iii) Seismic-related ground failure, including liquefaction? (Sources: 1, 14, 17) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: Liquefaction is a seismic phenomenon in which loose, saturated, fine-grained granular soils behave similarly to a fluid when subjected to high-intensity ground shaking. Liquefaction occurs when these ground conditions exist: 1) Shallow groundwater; 2) Low density, fine, clean sandy soils; and 3) High-intensity ground motion. These soils may acquire a high degree of mobility, which can lead to lateral movement, sliding, consolidation and settlement of loose sediments, sand boils and other damaging deformations. This phenomenon occurs only below the water table, but, after liquefaction has developed, it can propagate upward into overlying non-saturated soil as excess pore water dissipates. One of the primary factors controlling the potential for liquefaction is depth to groundwater. Typically, liquefaction has a low potential where groundwater is greater than 40-feet deep and is virtually unknown below 60 feet.

The subject site is located in an area of high to very high potential for liquefaction according to Figure EH-7 of the General Plan (1996) and the TGR Geotechnical Report. Other published data (State of California Seismic Hazard Zones Official Map, Newport Beach Quadrangle) from the California Division of Mines and Geology, published in April 7, 1997, indicates that the project site is located in an area identified as having a potential for soil liquefaction. However, the proposed improvements to the area primarily include surface RV parking and one 480 square foot modular building for daytime office use. As described in Section III a) ii) above, construction will comply with CBC standards to reduce impacts to less than significant.

- | | | | | |
|--------------------------------------|--------------------------|--------------------------|-------------------------------------|--------------------------|
| iv) Landslides? (Sources: 1, 14, 17) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--------------------------------------|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: According to the City of Huntington Beach General Plan, the site is not in an area susceptible to slope instability. The site is on a flat parcel of land and although a minor slope abuts the property to the east along the flood control channel, no significant slopes or other landforms susceptible to landslides exist in the vicinity of the property. Moreover, the California Division of Mines and Geology has not mapped any earthquake-induced landslides at, or in the vicinity of, the site which would be indicative of the potential for slope instability at or in the vicinity of the site. Less than significant impacts are anticipated.

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| b) Result in substantial soil erosion, loss of topsoil, or changes in topography or unstable soil conditions from excavation, grading, or fill? (Sources: 1, 17) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: The project and vicinity are urbanized and have relatively flat topography. The project site has been previously graded and developed with structures, parking surfaces and roadways. Although the proposed project has the potential to result in erosion of soils during construction activities, erosion will be minimized by compliance with standard City requirements for submittal of an erosion control plan prior to issuance of building permits, for review and approval by the Department of Public Works. Implementation of the proposed project would not require significant alteration of the existing topography of the project site. Less than significant impacts would occur and no mitigation measures would be required.

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on or off-site landslide, lateral spreading, subsidence, liquefaction or collapse? (Sources: 1, 14, 17)
-

Discussion: Refer to Responses III.a) iii) and III.a) iv) for discussion of liquefaction and landslides, respectively. Subsidence is large-scale settlement of the ground surface generally caused by withdrawal of groundwater or oil in sufficient quantities such that the surrounding ground surface sinks over a broad area. Withdrawal of groundwater, oil, or other mineral resources would not occur as part of the proposed project and, therefore, subsidence is not anticipated to occur. Adherence to the design recommendations of soils studies and grading plans as required by the City will ensure that no or less than significant geology related impacts will occur.

- d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property? (Sources: 1, 17)
-

Discussion: The project site is located in an area of moderate to high expansive soil conditions (Figure EH-12, City of Huntington Beach General Plan). Proposed improvements associated with the project include an asphalt parking area, a small modular office building, landscaping and perimeter fencing. All improvements will be required to comply with standard conditions of approval including submittal and approval of grading plans. All impacts from expansive soils are anticipated to be less than significant.

- e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of wastewater (Sources: 1, 5)
-

Discussion: The City of Huntington Beach Public Works Department has determined that the public sewer system can accommodate the proposed development. No septic tanks or alternative waste water disposal systems are necessary.

IV. HYDROLOGY AND WATER QUALITY. Would the project:

- a) Violate any water quality standards or waste discharge requirements? (Sources: 19, 20)
-

Discussion: Water quality standards and waste discharge requirements will be addressed in the project design and development phase pursuant to a Storm Water Pollution Prevention Plan (SWPPP) and Water Quality Management Plan (WQMP), prepared by a Civil or Environmental Engineer in accordance with the National Pollution Discharge Elimination System (NPDES) regulations and approved by the City of Huntington Beach Department of Public Works. The SWPPP and WQMP will establish Best Management Practices (BMPs) for construction and post-construction operation of the facility, including source, site and treatment controls to be installed and maintained at the site. The WQMP and SWPPP are standard requirements for development in the City of Huntington Beach, and with implementation, will ensure compliance with water quality standards and water discharge requirements, which will reduce project impacts to a level that is less than significant. A

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Preliminary WQMP has been submitted by the applicant, which identifies source, site and treatment controls for the reduction of pollutants to the surrounding water sources such as landscaped areas, use of low maintenance vegetation and installation of natural filter systems. As described in Section IV c-e) below, the applicant proposes to install three gravel infiltration pits to serve as both a water quality BMP and detention for 100-year storm events. Because the proposed RV storage site consists primarily of a parking lot for stored vehicles, leaking oil may occur and collect on the asphalt. The Preliminary WQMP proposes that run-off from the parking lot will enter a vegetative strip for pre-treatment prior to entering the gravel pit for percolation.

Final WQMP and SWPPP must be approved prior to issuance of grading permits. In addition, the Department of Public Works has identified numerous standard requirements applicable to the various project components, including the proposed parking areas and RV dump station to ensure no significant impact to water quality would occur.

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| b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted? (Sources: 19, 20) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: The Huntington Beach Public Works Department prepared a Master Plan for the City's water system in 2000. The Master Plan addresses water supply issues within the City and pertinent surrounding areas. The Utilities Division of the Public Works Department has reviewed the project plans and did not identify any concerns regarding impacts to ground water supplies or groundwater recharge due to the nature of the proposed uses. The project would likely result in a decrease in water consumption previously planned for in the Master Plan as the residential zoning designation will be eliminated and replaced with four specific uses. All four specific land uses are lower in water consumption rates than single family residential and, therefore, do not present a significant impact to water supplies. In addition, the project is subject to compliance with the City's Water Ordinance, including the Water Efficient Landscape Requirements, as well as Title 24 conservation measures such as low flow fixtures, which ensure water consumption is minimized. The estimated water demand for the proposed project can be accommodated by the City's water service capacity and does not represent a significant impact.

Based on the Fire Department's requirement for a private on-site fire hydrant system, a hydraulic water analysis is required to identify any off-site water improvements necessary to adequately protect the property per the Fire Department requirements. If necessary, the applicant shall be required to upgrade/improve the City's water system per Water Standards to meet the water demands to the site and/or mitigate the impacts of the property at no cost to the City. The applicant shall provide the City with a site plan showing the existing and proposed on-site and off-site water improvements (including pipelines sizes, fire hydrants, meters, and backflow device locations). The applicant shall be responsible to pay the City for performing the analysis using the City's hydraulic water model. Based on standard conditions of approval, the water model shall be completed prior to submittal of final parcel map. Less than significant impacts are anticipated.

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| c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
result in substantial erosion or siltation on or off-site? (Sources: 4, 5, 19, 20)				
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount or surface runoff in a manner which would result in flooding on or off-site? (Sources: 4, 5, 19, 20)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff? (Sources: 19, 20)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion: (c-e) The project site, in its existing condition, is almost entirely pervious with the landscape nursery business occupying the majority of the site. The proposed RV Storage facility will include the installation of approximately 10-11 acres of asphalt for the parking and access to the recreational vehicles stalls.

The existing drainage pattern of the site is divided into three sub-areas. One, E1, at the very entrance of the site along Ward Street is approximately 1 acre of self-contained drainage. This 1 acre drains towards two Edison towers located near the center of this drainage area. Drainage is collected and infiltrated into the gravel area surrounding each of these towers. The second drainage sub-area, E2, contains approximately 3.5 acres and is located at the northeast portion of the site. Much like sub-area E1, E2 drains toward two Edison towers located near the center of the sub-area where drainage is collected and infiltrated into the gravel surrounding each of the towers. The final drainage sub-area, E3, drains toward the east property line where it eventually infiltrates into the existing soil. Because there are no drainage devices located on the existing property, ponding does occur during larger storms.

The proposed post-development drainage pattern is very similar to the existing drainage pattern, except drainage will be directed away from the Edison towers and drainage devices will be added to the project to aid in the infiltration process. The proposed drainage pattern will be subject to City of Huntington Beach Department of Public Works approval. Area P1, at the very entrance of the site will use curb and gutter to direct drainage towards Ward Street. Area P2, containing the northeast portion of the site, will collect drainage through v-gutters that will then direct the flows to an infiltration trench located along the eastern property line of the project. The final drainage sub-area, P3, drains toward the east property line where an infiltration trench will collect all of the runoff from this sub-area.

The proposed grading for this project was designed to limit the offsite runoff to less than that of the existing condition, while adding Best Management Practices (BMPs) to achieve the necessary stormwater and water quality requirements.

The project site is bordered to the east by the Santa Ana River Channel maintained by the Orange County Flood Control District. No direct connection to the channel is proposed. Although the existing drainage pattern is expected to be altered during the construction phase, erosion and siltation during construction will be minimized to a less than significant level by employing Best Management Practices (BMPs) for erosion control, pursuant to a City approved Storm Water Pollution Prevention Plan (SWPPP) and Water Quality

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Management Plan (WQMP). Required SWPPP, WQMP and hydrology and hydraulic studies, to be submitted in accordance with City of Huntington Beach standard development requirements, will identify BMPs for construction and operation ensuring no significant impact associated with polluted runoff. Less than significant impacts are anticipated.

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| f) Otherwise substantially degrade water quality?
(Sources: 19, 20) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: See discussion under Section IV(a).

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| g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? (Sources: 5, 8) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
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Discussion: The proposed project site consists of electrical utilities, a landscape nursery and a proposed RV parking facility. No housing is proposed. The subject site is designated as Flood Zone X on the Flood Insurance Rate Map (FIRM), which is not subject to Federal Flood Development restrictions and no impacts are anticipated.

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| h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows? (Sources: 5, 8) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
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Discussion: The proposed project site is designated as Flood Zone X in the Flood Insurance Rate Map (FIRM), which is not subject to Federal Flood Development restrictions. The project site is not situated within the 100-year flood hazard area as mapped in the FIRM and no impacts are anticipated.

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| i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam? (Sources: 1, 8) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
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Discussion: The project site is not located within a flood hazard zone. The site, along with the entire eastern end of the city is bordered to the east by the Santa Ana River Channel maintained by the Orange County Flood Control District. While the project includes storage of recreational vehicles and one small office building, no on-site living or residential uses will be permitted. There are no impacts anticipated from the failure of a levee or dam.

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| j) Inundation by seiche, tsunami, or mudflow? (Sources: 1, 7, 14, 16) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
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Discussion: The elevation of the site above mean sea level (approximately 20 feet) and its distance from the ocean (approximately 3 ½ miles) and other large bodies of water suggest that the probability of experiencing adverse effects from tsunamis and seiches is low at the site. According to Figure EH-8 of the General Plan the site is not located within a potential tsunami run-up area and no impacts are anticipated.

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| k) Potentially impact stormwater runoff from construction activities? (Sources: 19, 20) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Discussion: See discussion under Section IV(a) and IV(e).

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| l) Potentially impact stormwater runoff from post-construction activities? (Sources: 19, 20) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: See discussion under Section IV(a) and IV(e).

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| m) Result in a potential for discharge of stormwater pollutants from areas of material storage, vehicle or equipment fueling, vehicle or equipment maintenance (including washing), waste handling, hazardous materials handling or storage, delivery areas, loading docks or other outdoor work areas? (Sources: 19, 20) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: The proposed RV parking lot will introduce vehicle fuels onto the project site in a different way than the landscape nursery previously used the site. The parked vehicles will contain vehicle fuel in the fuel tanks but no fueling systems are proposed in conjunction with the storage facility. A vehicle wash station and a dump station for gray and black water are proposed. The City of Huntington Beach Public Works Department has indicated that the vehicle wash station must be directed to the sanitary sewer (upon approval by the Orange County Sanitation District), to an engineered infiltration system, or to an equally effective alternative. The dump station must tie directly to the sanitary sewer. In accordance with standard City of Huntington Beach development requirements, hydrology and hydraulic studies for both on-site and off-site facilities, Storm Drain, Storm Water Pollution Prevention Plans (SWPPP) and Water Quality Management Plans (WQMP) conforming with the current National Pollution Discharge Elimination System (NPDES) requirements, prepared by a Licensed Civil Engineer, shall be submitted to the Department of Public Works for review and approval. Specific requirements and measures to be incorporated into the required studies and plans are identified in Attachment No. 4, City Policies, Standard Plans, and Code Requirements of the Huntington Beach Zoning & Subdivision Ordinance and Municipal Code. Refer to response in Section IV(a) for further discussion. Less than significant impacts are anticipated.

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| n) Result in the potential for discharge of stormwater to affect the beneficial uses of the receiving waters? (Sources: 19, 20) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: See discussion under Section IV(a) and IV(e).

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| o) Create or contribute significant increases in the flow velocity or volume of stormwater runoff to cause environmental harm? (Sources: 19, 20) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: See discussion under Section IV(e).

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| p) Create or contribute significant increases in erosion of the project site or surrounding areas? (Sources: 19, 20) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: See discussion under Section IV(e).

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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V. **AIR QUALITY.** The City has identified the significance criteria established by the applicable air quality management district as appropriate to make the following determinations. Would the project:

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| a) Violate any air quality standard or contribute substantially to an existing or projected air quality violation? (Sources: 9, 21) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: See discussion under Section V.e. below.

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| b) Expose sensitive receptors to substantial pollutant concentrations? (Sources: 4, 9, 21) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: See discussion under Section V.e. below.

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| c) Create objectionable odors affecting a substantial number of people? (Sources: 5, 6) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: See discussion under Section V.e. below.

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| d) Conflict with or obstruct implementation of the applicable air quality plan? (Sources: 9, 21) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: See discussion under Section V.e. below.

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| e) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? (Sources: 9, 21) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion:

Discussion a – e: The proposed project consists of development of an approximately 13-acre parcel for a 557-space RV storage facility and associated site improvements. The City of Huntington Beach is located within the South Coast Air Basin, which is regulated by the South Coast Air Quality Management District (SCAQMD). The entire Basin is designated as a national-level nonattainment area for Ozone, Carbon Monoxide (CO), respirable particulate matter (PM₁₀) and fine particulate matter (PM_{2.5}). The Basin is also a State-level nonattainment area for Ozone, PM₁₀ and PM_{2.5}. Sensitive receptors in the area include residents in nearby developments west as well as recreation users of the trail to the east and park and school uses to the southwest. The proposed zoning text amendment will not have any impacts on air quality.

Impacts from objectionable odors could potentially occur during construction of the project. However, impacts would be intermittent and short-term and would not persist once construction was completed. Vehicle storage uses in general are not sources of objectionable odors. Potential odors would be limited to the gray and black water dump station, which is tied by vacuum seal directly to the sanitary sewer and located at the

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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northeast corner of the property, away from sensitive uses. Other potential odors would be limited to typical office use wastes, which are stored in refuse containers and picked up on a weekly basis. As such, impacts from odors would be less than significant.

The 2007 Air Quality Management Plan (AQMP) is the region’s applicable air quality plan and was prepared to accommodate growth, to reduce the high levels of pollutants within the areas under jurisdiction of the SCAQMD, to return clean air to the region, and minimize the impact on the economy. Projects that are considered to be consistent with the General Plan are considered to be consistent with the AQMP. Although the proposed project is proposing a general plan amendment to add the specific plan overlay, there is no additional growth in population size and no additional residential units as a result of the project. Therefore, the proposed project would not conflict with the AQMP and impacts would be less than significant.

Short-term: The construction of the project may result in short-term air pollutant emissions from the following activities: the commute of workers to and from the project site; dismantling of shade structures at the landscape nursery; relocation of boxed nursery material from Planning Area 1 to Planning Area 3; grading activities; delivery and minimal hauling of construction materials and supplies to and from the project site; fuel combustion by on-site construction equipment; and dust generating activities from soil disturbance. Emissions during construction were calculated using URBEMIS2007 program (version 9.2.4). The allotment of equipment to be utilized during each phase was based on defaults in the URBEMIS2007 program and was modified as needed to represent the specifics of the proposed project.

The URBEMIS model calculates total emissions, on-site and offsite, resulting from each construction activity which are compared to the SCAQMD Regional Thresholds. A comparison of the project’s total emissions with the regional thresholds is provided below. A project with daily construction emission rates below these thresholds is considered to have a less than significant effect on regional air quality.

SCAQMD Regional Pollutant Emission Thresholds of Significance - Construction						
	Regional Significance Threshold (Lbs/day)					
	CO	VOC	NOx	PM ₁₀	PM _{2.5}	SO ₂
Estimated Construction Emissions for proposed project	26.89	6.07	50.05	91.76	20.95	0.01
Significance Threshold	550	75	100	150	55	150
Exceed Threshold?	NO	NO	NO	NO	NO	NO

Based on the aforementioned table construction of the project would not exceed the regional emissions thresholds nor would it expose sensitive receptors to substantial pollutant concentrations. Therefore, a less than significant impact is anticipated.

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SCAQMD Regional Pollutant Emission Thresholds of Significance – Operations						
	Regional Significance Threshold (Lbs/day)					
	CO	VOC	NO _x	PM ₁₀	PM _{2.5}	SO ₂
Estimated Operational Emissions for proposed project	270.91	21.21	51.07	1.85	1.13	0.25
Significance Threshold	550	55	55	150	55	150
Exceed Threshold?	NO	NO	NO	NO	NO	NO

Long-term: Post-construction emissions were also calculated using the URBEMIS2007 program version (9.4.2). The program was set to calculate emissions for the proposed 557-space RV storage lot. The default URBEMIS2007 variables were used for the calculations.

Based on the aforementioned table post-construction emissions from the proposed project would not exceed the regional thresholds nor would it expose sensitive receptors to substantial pollutant concentrations. Therefore, a less than significant impact is anticipated.

In addition, the project does not come close to exceeding established thresholds for any pollutant including the identified nonattainment pollutants (Ozone, CO, PM₁₀ and PM_{2.5}) and ozone precursors (NO_x and VOC) both for construction and post-construction and therefore, would not contribute a cumulatively considerable increase in these pollutants.

Greenhouse Gases

AB 32 codifies the state’s goal to reduce its global warming by requiring that the state’s greenhouse gas (GHG) emissions be reduced to 1990 levels by 2020. This reduction will be accomplished through an enforceable statewide cap on greenhouse gas emissions that will be phased in starting in 2012. In order to effectively implement the cap, AB 32 directs the California Air Resources Board (CARB) to develop appropriate regulations and establish a mandatory reporting system to track and monitor greenhouse gas emissions levels. In addition, the State Office of Planning and Research (OPR) has until January 1, 2010 to adopt CEQA guidelines for evaluation of greenhouses gases. A draft of the proposed amendments to the CEQA guidelines was released in April 2009 and states that a local agency must develop its own significance criteria based on local conditions, data and guidance from other sources.

The proposed project would result in a total of approximately 34.19 tons of CO₂ emissions during construction. Post-construction CO₂ emissions would be approximately 4,502.22 tons/year. Therefore, the project would produce GHG emissions. Other GHG emissions could result from increases in electricity and natural gas usage and solid waste production, all of which would minimally occur with the proposed project. Although, the amount of post-construction GHG emissions from the project (4,502.22 tons/yr) represents a negligible percentage of the overall state of California GHG emissions (484,400,000 tons/yr - 2004), since there are no thresholds of significance established yet, any contribution of GHG emissions can be considered significant.

The proposed project incorporates design features that promote energy efficiency and a reduction in GHG emissions, both directly and indirectly. For instance, the project is proposing to utilize a storm drain system designed to capture high-volume and low-volume flows and allow them to percolate into the ground thereby reducing the amount of water that enters the storm drain system and drought tolerant landscaping. In

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addition, the project is required to comply with all applicable City codes and requirements pertaining to energy efficiency and water use efficiency as well as applicable requirements for construction equipment that would limit truck and equipment idling times, exhaust and dust. The identified project design features and applicable requirements are consistent with the GHG reduction strategies recommended by the California Climate Action Team (CCAT), the California Air Pollution Control Officers Association (CAPCOA) and the California Attorney General's office. Therefore, due to the project's small contribution to GHG emissions in addition to project design features that would reduce GHG emissions, impacts would be less than significant.

VI. TRANSPORTATION/TRAFFIC. Would the project:

- a) Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (e.g., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections?) (Sources: 1, 11, 22)

Discussion: The project's potential to generate a substantial increase in traffic was assessed by comparing a similar RV Storage facility in Anaheim (Anaheim RV Storage) owned and operated by the proponent. The Huntington Beach facility is proposed to operate similarly to the Anaheim facility with the difference being capacity. The Anaheim facility has a capacity for 288 RV spaces while the Huntington Beach facility proposes 557 spaces or just under double the size.

The analysis concludes that the proposed project is anticipated to generate approximately 160 net new weekday daily vehicle trips, a maximum of 12 net new weekday AM peak hour vehicle trips, and a maximum of 20 net new weekday PM peak hour vehicle trips. The weekend operations result in an average of approximately 58 vehicle trips generated per weekend day with an average of three AM peak hour trips and 10 PM peak hour trips. The projected traffic generation rates do not exceed the applicable City of Huntington Beach and State of California Department of Transportation Engineers (Caltrans) traffic impact thresholds (100 peak hour trips) for requiring a Traffic Impact Study. Accordingly, based on City of Huntington Beach and Caltrans traffic impact analysis guidelines, no significant increase in traffic is expected as a result of the proposed project.

Based on information in the City's General Plan and Department of Public Works, Traffic Division, the adjacent intersection (Garfield Avenue and Ward Street) currently operates at LOS B during the AM peak hour and LOS C during the PM peak hour. The segment of Ward Street between Yorktown Avenue and Garfield Avenue, operates at LOS B, while the segment of Garfield Avenue between Brookhurst Street and Ward Street operates at LOS C. The City's current policy for acceptable level of service at traffic-controlled intersections is LOS D; and LOS C for roadway segment links. This project is forecast to generate 160 new daily trips, which will not result in a change in LOS at any of the surrounding intersections or street segments and they will continue to operate at acceptable LOS standards. The project will be subject to payment of traffic impact fees for each net new added daily trip.

Construction traffic resulting from development of the project may result in short-term interruptions to traffic circulation, including pedestrian and bicycle flow. Additionally, relocation of shade structures and potted landscape materials from Planning Area 1 to Planning Area 3 will result in an increase in truck trips on a short term basis. The relocation of the nursery and consolidation of nursery operations within Planning Area 3 is expected to last approximately one week. Based on the scope of the RV storage construction, approximately

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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two months duration, the short-term interruptions to traffic are not considered to be significant.

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| b) Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways? (Sources: 1, 11, 22) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: Please refer to discussion item VI (a) above.

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| c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks? (Sources: 12) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
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Discussion: Although the City is located within the Planning Area for the Joint Forces Training Center in Los Alamitos, the project site is not located within the height restricted boundaries identified in the Airport Environs Land Use Plan or within two miles of any known public or private airstrip. The proposed project does not propose any structures with heights that would interfere with existing airspace or flight patterns. No impacts would occur.

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| d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses? (Sources: 5) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: The project site is located at the intersection of two existing arterial highways that provide access to the site. No new roadways or intersections are proposed although improvements to Ward Street will be required. The project is subject to compliance with City standards for vision clearance at street/driveway intersections, minimum drive aisle widths and truck turning radii designed to ensure hazards are minimized, as well as a Traffic Construction Mitigation Plan during construction of the project. The project's proposed driveway access and on-site and off-site circulation has been evaluated by the Departments of Fire, Planning and Public Works and with the required Ward Avenue improvements, found to be consistent with City standards for safe access and circulation.

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| e) Result in inadequate emergency access? (Sources: 5) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
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Discussion: The proposed site plan has been reviewed by the Departments of Fire and Public Works for conformance with City requirements for emergency access. The project's proposed driveway access and on-site circulation has been found to be consistent with City standards for emergency access and circulation. During construction, construction equipment and construction worker's vehicles will be contained on-site and will not block streets or potentially impede emergency access. Less than significant impacts are anticipated.

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| f) Result in inadequate parking capacity? (Sources: 2, 5, 23) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Discussion: The proposed RV storage project results in a low demand for parking spaces. Recreational vehicle users typically drive a passenger vehicle to the lot, pull out the recreational vehicle and park the passenger vehicle in the space until return from their trip. Alternatively, vehicles may enter the site, hook-up the recreation vehicle trailer or toy hauler and leave the site. Therefore, additional parking spaces for the vehicle storage area are not necessary. Parking adjacent to the office building for visitors, potential customers, one employee, and patrolling security personnel is necessary. Accordingly, five parking spaces are provided adjacent to the office building and will be sufficient for the anticipated demand. The proposed project complies with parking requirements specified in the Ward Garfield Specific Plan and will not result in an inadequate parking capacity. No variances to parking standards are proposed or required, and no unique circumstances exist that would suggest that the minimum parking standards applicable to the project are inadequate. Accordingly, no parking related impacts are anticipated.

- g) Conflict with adopted policies supporting alternative transportation (e.g., bus turnouts, bicycle racks)? (Sources: 1, 2, 23)

Discussion: The proposed development does not exceed the thresholds established in Section 230.36 of the HBZSO – *Transportation Demand Management*, based on employment generation factors contained therein and the applicant's employment projections. The thresholds established in Section 230.36 serve as a basis to identify projects with the potential for significant traffic and air quality impacts and which warrant implementation of transportation demand strategies. According to Section 231.20 of the HBZSO, the proposed RV storage project would require 23 bicycle parking stalls. However, that standard is based on commercial and industrial business with up to 50,000 square feet of building area and one bike stall is required for every 25 parking spaces. In this case, a high demand of bicycle riders to the RV storage lot is not anticipated, therefore, the minimum of three bicycle racks as required by the HBZSO should be sufficient. No impacts are anticipated.

VII. BIOLOGICAL RESOURCES. Would the project

- a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? (Sources: 1, 10, 18)

Discussion: The project site and all surrounding properties are currently developed with quasi-public, residential, and public park land uses. The project site does not support any unique, sensitive, or endangered species and is not shown in the General Plan as a generalized habitat area.

The applicant commissioned a Fairy Shrimp Habitat Assessment by Bonterra Consulting (December 1, 2009) to determine whether the project site supported potential fairy shrimp habitat. The assessment concluded that there was no natural ponding identified that would support fairy shrimp, and that other than runoff from the existing landscape nursery irrigation system, there was no substantial habitat to support the Riverside or San Diego fairy shrimp. Less than significant impacts to biological resources are anticipated.

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or US Fish and Wildlife Service? (Sources: 1, 10)

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The project site is currently occupied by the Ellis Substation and Village Nursery Landscape Center. The project site does not contain any riparian habitat or sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or US Fish and Wildlife Service. The project will not result in any loss to endangered or sensitive animal or bird species and does not conflict with any habitat conservation plans.

See Discussion VII. (a) above. Less than significant impacts are anticipated.

- c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means? (Sources: 1, 10)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: The project does not contain any wetlands; therefore, no impacts are anticipated.

- d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors or impede the use of native wildlife nursery sites? (Sources: 1, 10)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: The project area is surrounded by residential and quasi-public uses. The site does not support any fish or wildlife and will not interfere with the movement of any fish or wildlife species nor impede the use of native wildlife nursery sites.

- e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance? (Sources: 1, 2, 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: The site contains an existing landscape nursery that will be relocated to a smaller area of the site. The site does not contain any trees which may be considered mature and which could be impacted by construction. Construction of the project will be subject to standard requirements for the submittal of a landscape plan. No other biological resources exist on the site.

- f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan? (Sources: 1, 10)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Discussion: The project site does not support any unique or endangered plant or animal species and is not a part of any adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan; therefore, no impacts to any habitat or wildlife area are anticipated.

VIII. MINERAL RESOURCES. Would the project:

- a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state? (Sources: 1)

Discussion: The proposed project will not result in the loss of a known mineral resource. The project site is not designated as a known mineral resource recovery site in the General Plan. No impacts are anticipated.

- b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan? (Sources: 1)

Discussion: The project site is not designated as an important mineral resource recovery site in the General Plan or any other land use plan. Development of the project is not anticipated to have any impact on any mineral resource recovery. No impacts to mineral resources are anticipated.

IX. HAZARDS AND HAZARDOUS MATERIALS.

Would the project:

- a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials? (Sources: 3, 6, 10)

Discussion: The SCE substation, towers, and electrical transmission lines will remain in place and continue to operate as they do today. The landscape nursery will be consolidated from its current location on the site to a smaller portion of the site and will then continue its existing operation. The RV storage project will not engage in the routine transport, use, or disposal of hazardous materials. However, the stored vehicles will contain vehicle fuel in their fuel tanks and the site will offer a vacuum sealed dump station for gray and black water. The dump station will be tied directly to the sanitary sewer, subject to approval by the Orange County Sanitation District. In any event, all hazardous materials use and storage are subject to review by the Departments of Building & Safety and Fire in conjunction with standard building permit and certificate of occupancy inspection processes. Refer to Section IV a) and c-e) for further discussion on hazardous material related to water quality. Less than significant impacts are anticipated.

- b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment? (Sources: 1, 6, 13)

Discussion: The project site is not located in a methane district. No impacts resulting from hazards involving the release of hazardous materials during construction or during operation of the facility is anticipated.

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- c) Emit hazardous emissions or handle hazardous or acutely hazardous material, substances, or waste within one-quarter mile of an existing or proposed school? (Sources: 1, 4)
-

Discussion: The southern portion of the project site is located adjacent to Arevalos Park, which abuts the existing Pegasus School site. The SCE substation, towers, and electrical transmission lines will remain in place and continue to operate as they do today. The landscape nursery will be consolidated from its current location on the site to a smaller portion of the site and will then continue its existing operation. No acutely hazardous emissions, substances or waste will be emitted or handled during operations of the RV Storage facility. Development of the RV storage lot and consolidation activities of Village Nurseries will be required to comply with Fire Department specifications. Vehicle fuel will be present on-site while the business is in operation, however, it will be contained within the parked vehicles, no on-site fueling station is proposed as part of the facility, and less than significant impacts are anticipated.

- d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment? (Sources: 1, 13)
-

Discussion: The site is not listed on the State’s Hazardous Waste and Substances Site List. No impacts are anticipated.

- e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? (Sources: 10, 12)
-

Discussion: Although the City is located within the Planning Area for the Joint Force Training Center, Los Alamitos, the project site is not located within the height restricted boundaries identified in the Airport Environs Land Use Plan or within two miles of any known public or private airstrip. The proposed project does not propose any structures with heights that would interfere with existing airspace or flight patterns. No impacts would occur.

- f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area? (Sources: 10, 12)
-

Discussion: The project site is not located near any private airstrips. No impacts are anticipated.

- g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? (Sources: 1, 16)
-

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Discussion: The project has been reviewed by the Fire Department and is designed to be in compliance with fire access and circulation requirements. The specific plan and proposed development of a portion of the site will not interfere or conflict with an adopted emergency response plan or evacuation plan. No impacts are anticipated to any emergency response or evacuation plans.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| h) Expose people or structures to a significant risk of loss, injury, or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?
(Sources: 1) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The project is located in an urbanized area and is not near any wildlands. No impacts are anticipated.

X. NOISE. Would the project result in:

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies? (Sources: 1, 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: Refer to Section (d) below.

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?
(Sources: 1, 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: Refer to Section (d) below.

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project? (Sources: 1, 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: Refer to Section (d) below.

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project? (Sources: 1, 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: a) – d)

The SCE substation, towers, and electrical transmission lines will remain in place and continue to operate as they do today. The landscape nursery will be consolidated from its current location on the site to a smaller portion of the site and will then continue its existing operation.

The RV storage project will generate short-term noise impacts during construction, including noise generated

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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by earth-moving equipment, trucks and power tools. However, the project will be subject to compliance with Chapter 8.40 – *Noise*, of the Huntington Beach Municipal Code which restricts all construction activities to the hours between 7:00 AM and 8:00 PM Monday - Saturday. Construction activities are prohibited Sundays and Federal holidays. Accordingly, construction related noise impacts are expected to be less than significant.

The project's potential to generate noise related impacts after completion of construction include the vehicle trips to and from the RV storage facility. The noise levels from the RV's are anticipated to be below City of Huntington Beach daytime (7:00AM to 10:00PM) noise ordinance limits for anticipated daytime noise sources.

The facility will not be in operation during nighttime hours. Standard office hours are 8:00AM to 5:00PM (Sunday-Friday); 7:00AM to 6:00PM (Saturday). Remote gate hours will be programmed from 5:00AM to 7:00PM, allowing tenants to enter the site to store or retrieve vehicles. The tenants will access the facility with a remote control transmitter to open the gate while still on Ward Street. The remote system allows efficient access to the site and eliminates vehicle idling noise within the driveway approach at the site entrance. No amplified or pager system is proposed, therefore minimizing annoyance to adjacent residences. Additionally, no vehicular repair or living within the vehicles will be permitted. Noise impacts are expected to be less than significant.

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|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? (Sources: 10, 12) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The City of Huntington Beach is included in the Planning Area for the Joint Forces Training Center in Los Alamitos. However, the site is located a considerable distance from the Training Center, such that the project would not be impacted by flight activity and noise generation from the Center. No impacts are anticipated.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels? (Sources: 10, 12) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The project is not located within the vicinity of a private airstrip. No impacts are anticipated.

XI. PUBLIC SERVICES. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

- | | | | | |
|---------------------------------------|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a) Fire protection? (Sources: 1, 16) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b) Police Protection? (Sources:1, 16) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Discussion: a)-b) The proposed project has been reviewed by Huntington Beach Fire Department and Police Department staff. The project site is located approximately one and one-half miles from the Bushard Fire Station and approximately three miles from the Main Police Station. Estimated emergency first response times are within the 80 percent/Five minute response time objective established in the City's Growth Management Element. Estimated emergency first response times from the Main Police Station are within acceptable service levels. The proposed project can be adequately served by existing Fire and Police protection service levels. Because the site is currently developed with a landscape nursery and electrical utilities, the City already provides service. The addition of the RV storage lot to the specific plan area is expected to slightly increase service demand for the project site. However, this increase is expected to be minimal and less than significant impacts are anticipated.

- c) Schools? (Sources: 1)

Discussion: The project will provide for continued operation of existing uses on the project site and the proposed RV Storage facility. No significant increase in the number of persons employed at the site is planned or anticipated. The applicant will also be required to pay school district fees for the net increase in the floor area proposed. Based on the negligible increase of employees and the requirement for payment of school fees, less than significant impacts are anticipated.

- d) Parks? (Sources: 1)

Discussion: The project is not expected to have any significant impact on park facilities, since the proposed project will provide for operation of low level commercial uses and existing utilities. No significant increase in the number of people employed at the site is planned or anticipated. The project will be subject to payment of park fees in compliance with the HBZSO. No significant impacts to park services are anticipated.

- e) Other public facilities or governmental services?
(Sources: 1)

Discussion: The project has been reviewed by the various City Departments, including Public Works, Building and Safety, Fire, Police and Planning for compliance with all applicable City codes. With implementation of recommended conditions of approval, and compliance with City specifications, no significant impacts to public services are anticipated.

XII. UTILITIES AND SERVICE SYSTEMS. Would the project:

- a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?
(Sources: 1)

- b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? (Sources: 1, 10, Dept. of Public Works)

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? (Sources: 1, 10, Dept. of Public Works)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? (Sources: 1, 10, Dept. of Public Works)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? (Sources: 1, 10, Dept. of Public Works)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs? (Sources: 1, 10)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste? (Sources: 1, 10)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h) Include a new or retrofitted storm water treatment control Best Management Practice (BMP), (e.g. water quality treatment basin, constructed treatment wetlands?) (Sources: 19, 20)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Discussion: a)-h) The project will provide for the continued operation of the Ellis Substation and the Village Nurseries Landscape Center together with the establishment of a new RV Storage facility. The new RV facility is expected to result in a decrease in the total number of people employed at the site as the size of the landscape nursery will be reduced by approx. 14 acres. There will not be an increase the amount of wastewater or solid waste generated at the site, however, the City of Huntington Beach Public Works Department has determined that currently there are no public sanitary sewer facilities on Ward Street in the project vicinity to serve the development. A public sanitary sewer main shall be designed and constructed to run southerly on Ward Street to Sunday Drive and turn westerly to connect to the manhole just west of the intersection of Ward Street and Sunday Drive.

The developer shall be required to submit a hydrology and hydraulic study for both on-site and off-site facilities and a project WQMP for review and approval by the Public Works Department. The studies and the proposed drainage improvements shall include on-site, privately maintained Best Management Practices (BMPs) to control the quality of run-off water from the development. All utility connections to the project site will be in accordance with applicable City standards.

Solid waste collection service for the City of Huntington Beach is provided by Rainbow Disposal, under an exclusive long-term contract with the City. Collected solid waste is transported to a transfer station where the solid waste is sorted and processed through a Materials Recovery Facility where recyclable materials are removed. The remaining solid waste is transferred to the Frank R. Bowerman Landfill located in the City of Irvine. The landfill has a remaining capacity in excess of 30 years based on the present solid waste generation rates, and the project is not expected to generate a substantial amount of daily waste products in the long term nor as a result of construction. Based on this and the nature of uses proposed, the project is not anticipated to noticeably impact the capacity of existing landfills that will serve the use and the project will be required to comply with federal, state, and local statutes and regulations related to solid waste.

The 2000 City of Huntington Beach Water Master Plan analyzes demands and anticipated impacts of future developments based on the Land Use Element designations. The proposed project includes a request to amend the General Plan, establish a specific plan designation, and eliminate the current RL Low Density Residential zoning designation. The proposed specific plan limits the allowable on-site land uses to four distinct uses: electrical substation and utilities, landscape nursery, RV storage, and wireless communication facilities. These limited uses result in a significantly less demand on water resources than the approximately 91 single family residential units that would be permitted under the current zoning. Less than significant impacts are anticipated.

XIII. AESTHETICS. Would the project:

- a) Have a substantial adverse effect on a scenic vista?
 (Sources: 1)

Discussion: The proposed project site is not situated adjacent to or in the vicinity of any scenic vista designated by the City or the State. As a result, no impacts are expected.

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? (Sources: 1)
-

Discussion: The site is not adjacent to a state scenic highway, nor are there any significant trees, rock outcroppings, or historic buildings in the vicinity of the subject site. No impacts will result.

- c) Substantially degrade the existing visual character or quality of the site and its surroundings? (Sources: 5)
-

Discussion: The site is adjacent to the existing Mariner’s Pointe mobile home park and existing single family residences that currently view the landscape nursery and electrical transmission towers and lines. The SCE substation will continue its current operation and will not result in any changed visual character for the surrounding uses. The Village Nurseries business will be consolidated from its current location to a smaller portion of the site and will also not result in a changed visual character.

The proposed RV Storage facility includes the installation of approximately eight ft. high ornamental fencing along the residential properties as well as a five foot wide landscape buffer. To minimize the view impacts to the residences, the proponent proposes to install intensified landscaping material to grow through the fencing and the residential units are currently surrounded by an existing six foot high masonry wall. While large profile recreational vehicles including motor homes and boats may be present on site, the landscape buffer and eight ft. high ornamental fence will serve to screen the view of the vehicles. Additionally, the site is only minimally visible from the surrounding public street system on Ward Street and the primary view of the property is currently large SCE towers. No chain-link fencing is proposed.

One 480 sq. ft. modular office building with a maximum height of 18’-6” is proposed to be centrally located on the RV storage property and is not anticipated to result in view impacts to the residents. Increased landscaping is proposed at the entry of the RV facility, concentrated along Ward Avenue, in front of the security gate and fencing system. Less than significant impacts are anticipated.

- d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area? (Sources: 1)
-

Discussion: The site is presently occupied by the Ellis Substation and Village Nurseries Landscape Center. The proposed RV facility will introduce security lighting along the perimeter of the facility. The proposed lighting uses “Dark Sky” technology that is designed to direct light down toward the ground and prevent light spillage onto the adjacent residential properties. The project will be subject to compliance with City codes requiring that lighting be shielded and directed so as to prevent glare and spillage onto adjacent residential properties. No significant impacts are anticipated.

XIV. CULTURAL RESOURCES. Would the project:

- a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5? (Sources:)
-

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
1, 2)				
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5? (Sources: 1, 2)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site unique geologic feature? (Sources: 1, 2)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries? (Sources: 1, 2)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion: a) – d) The project is not located in the vicinity of any known archeological, historic or other cultural resource. The site does not include any historic structures, no archaeological or paleontological resources have been identified, and the site has previously been disturbed and graded. No impacts are anticipated.

XV. RECREATION. Would the project:

a) Would the project increase the use of existing neighborhood, community and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? (Sources: 1, 2, 10)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The project consists entirely of low-level commercial development and existing utilities. No residential uses are proposed. There are fewer employees anticipated with the new RV facility than with the existing nursery. Consequently, no increase in resident or daytime population with the potential to significantly impact use of parks or other recreational facilities would occur.

b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment? (Sources: 1, 2, 10)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The specific plan does not include any recreational facilities or directly require the construction or expansion of recreational facilities. The RV storage project will be subject to payment of the City's park fee pursuant to the HBZSO. Payment of the park fee is considered a fair share contribution towards the development of additional recreational facilities in the City and serves to offset any project impacts.

c) Affect existing recreational opportunities? (Sources: 1, 2, 4, 10)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The project includes development of a RV Storage facility on a site currently developed with an electric substation and landscape nursery. No existing recreational opportunities exist on the site and none will be affected by the proposed project. The site is adjacent to Arevalos Park to the south and a bicycle and walking trail at the top of the slope to the east along the Santa Ana River Channel. The proposed project will

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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not interfere with the existing recreational trail and will not impede access to the trail as none currently exists within the project site. Two SCE towers are located between the proposed RV storage lot and Arevalos Park and serve as a buffer between the two uses. During construction of the RV storage lot, all construction materials and equipment will be contained within the project site area although construction noise may be a temporary impact to Arevalos Park. Due to the short two month construction duration, less than significant impacts are anticipated.

XVI. AGRICULTURE RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. Would the project:

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use? (Sources: 1, 2, 4, 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Conflict with existing zoning for agricultural use, or a Williamson Act contract? (Sources: 1, 2, 4, 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use? (Sources: 1, 2, 4, 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion: a) – c) The project site does not serve as farmland and is not identified as farmland on the State’s Important Farmlands map. The project will not impact property that was used for agriculture in the past, nor could the subject site be potentially utilized for agricultural purposes in the future based on its current residential zoning designation and utility and landscape nursery use. No impacts are anticipated.

XVII. MANDATORY FINDINGS OF SIGNIFICANCE.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? (Sources: 1) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Discussion: The proposed project is not situated within or in the vicinity of a wildlife resource habitat. As analyzed in this initial study, the project is located in areas previously developed that do not support any unique, sensitive, or endangered species. No impacts to any habitat or wildlife area are anticipated.

- b) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.) (Sources: 1-24)
-

Discussion: No impacts beyond those anticipated in the General Plan DEIR, which considers the potential cumulative impacts of projects anticipated under the current General Plan Land Use designations, are expected. Although the zoning of the site is proposed to change from Residential Low Density to Specific Plan No. 16, the General Plan land use designation will remain Public. The proposed specific plan overlay will limit permitted uses to four specific uses: electrical substation and utilities, landscape nursery, recreational vehicle storage, and wireless communication facilities. The project is proposed in an urbanized area with limited development potential. Consequently, no significant cumulative impact resulting from the proposed project when viewed in connection with probable future projects is anticipated.

- c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly? (Sources: 1-24)
-

Discussion: As discussed above in Sections I through XVI, no significant impacts that may cause substantial adverse effects on humans, associated with the project, are anticipated.

XVIII. EARLIER ANALYSIS.

Earlier analyses may be used where, pursuant to tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c)(3)(D).

Earlier Documents Prepared and Utilized in this Analysis:

<u>Reference #</u>	<u>Document Title</u>	<u>Available for Review at:</u>
1	City of Huntington Beach General Plan	City of Huntington Beach Planning Dept., Planning/Zoning Information Counter, 3rd Floor 2000 Main St. Huntington Beach
2	City of Huntington Beach Zoning and Subdivision Ordinance	"
3	City of Huntington Beach Municipal Code	"
4	Project Vicinity Maps	See Attachment #1 <i>See Study Session Staff Report for updated attachments</i>
5	Reduced Project Plans	See Attachment #2
6	Project Narrative	See Attachment #3 "
7	City of Huntington Beach Geotechnical Inputs Report	City of Huntington Beach Planning Dept., Planning/Zoning Information Counter, 3 rd Floor 2000 Main St. Huntington Beach
8	FEMA Flood Insurance Rate Map (February 18, 2004)	"
9	CEQA Air Quality Handbook South Coast Air Quality Management District (1993)	"
10	City of Huntington Beach CEQA Procedure Handbook	"
11	Trip Generation Handbook, 7 th Edition, Institute of Traffic Engineers	"
12	Airport Environs Land Use Plan for Joint Forces Training Base Los Alamitos (Oct. 17, 2002)	"
13	Hazardous Waste and Substances Sites List	"
14	State Seismic Hazard Zones Map	"

<u>Reference #</u>	<u>Document Title</u>	<u>Available for Review at:</u>
15	Huntington Beach Water Master Plan, December 2000	"
16	City of Huntington Beach Emergency Management Plan	"
17	Limited Geotechnical Investigation and Feasibility Percolation Study for Proposed RV Parking prepared by TGR Geotechnical, Inc. (August 17, 2009)	"
18	Results of Fairy Shrimp Habitat Assessment prepared by Bonterra Consulting (December 1, 2009)	"
19	Preliminary WQMP prepared by Blue Peak Engineering, Inc. (August 10, 2009)	"
20	Preliminary Hydrology Study prepared by Blue Peak Engineering, Inc. (August 10, 2009)	"
21	Ur URBEMIS Air Quality Assessment (December 2009)	"
22	Door King Traffic Counter Anaheim RV Storage Facility (July 2009)	"
23	Draft Ward Garfield Specific Plan No. 16	"
23	City Policies, Standard Plans and Code Requirements	"

~~Attachment #4~~
 see study session
 staff report for
 updated attachments