

Chapter 1 - Introduction

1.1. Purpose and Intent

The purpose of the Huntington Beach Downtown Specific Plan No. 5 (hereafter, “Specific Plan”) is to create a unique and identifiable downtown for Huntington Beach that capitalizes on the unique location and features of the City’s beachside downtown and is an economically vibrant, pedestrian-oriented destination for residents and visitors alike. This 2009 Specific Plan is an update to the 1983 Downtown Specific Plan and is part of a City-lead effort to ensure that planning regulations and guidelines provide direction with flexibility to accommodate both current and future development opportunities in the downtown area.

A Specific Plan is a regulatory tool that local governments use to implement the City’s General Plan and to guide development in a localized area. While the General Plan is the primary guide for growth and development in a community, a Specific Plan is able to focus on the unique characteristics of a special area by customizing the planning process and land use regulations to that area. A Specific Plan is enacted pursuant to Section 65450 et seq of the California Government Code.

An overall goal of the Specific Plan is to promote orderly and viable development that meets the community’s vision for the future of the downtown. This Specific Plan provides refinements to existing regulations where necessary to further the community goals and vision.

1.2. Boundary

The Specific Plan covers an area of 336 acres in the traditional and historic heart of the City of Huntington Beach. A detailed description of the exact Specific Plan boundary can be found in Appendix A. Generally, the Specific Plan Area extends from the intersection of Goldenwest Street with Pacific Coast Highway and curves along the coastline, including the Huntington Beach Pier, down to Beach Boulevard. The inland boundary of the Specific Plan Area follows the prolongation of Sunrise Drive from Beach Boulevard to Pacific View Avenue where the boundary curves along Huntington Street and Atlanta Avenue. From Atlanta Avenue, the boundary flows along Orange Avenue and continues up Lake Street to Palm Avenue where it connects over to Main Street and along Pecan Avenue to link down along 6th Street. From 6th Street, following along Walnut Avenue to Goldenwest Street, parcels within the first block adjacent to Pacific Coast Highway are included in the Specific Plan Area. All boundary lines follow the centerline of the affected street. Figure 1-1 shows the location of the Specific Plan Area within the City of Huntington Beach.

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Story

That portion of a building included between the surface of any floor and the surface of the floor or finished undersurface of the roof directly above it. A rooftop deck with walls less than or equal to 42 inches in height and covered access thereto shall not be counted as a story.

Suite hotel

A building designed for or occupied as a temporary lodging place which contains guest rooms and may contain kitchenettes and a separate living room for each unit.

Tieback

Horizontal wire or rod used to reinforce retaining walls for temporary or permanent shoring. With one end of the tieback secured to the wall, the other end is anchored to a stable structure, such as a concrete deadman which has been driven into the ground. The tieback-deadman structure resists forces that would otherwise cause the wall or structure to lean.

Travel services

Establishments providing travel information and reservations to individuals and businesses. This classification excludes car rental agencies.

Ultimate right-of-way

The most lateral edge of the area dedicated for street, utilities or alley purposes.

Usable open space

Outdoor or unenclosed areas on the ground or on a balcony, deck, porch, or terrace that is designed and accessible for outdoor living, recreation, pedestrian access or landscaping. Usable open space does not include parking facilities, driveways, utility or service areas, any required front or street side yard, any space with a dimension of less than 6 feet in any direction or an area of less than 60 square feet.

Visitor-serving facilities

Public and private developments that provide accommodations, food, and services, including hotels, motels, timeshares, campgrounds, restaurants, retail sales, cultural uses, and amusement areas for tourists.

Wetland

Lands within the coastal zone which may be covered periodically or permanently with shallow water and include saltwater marshes, fresh water marshes, open or closed brackish water marshes, swamps, mudflats and fens.

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3.2.4. Encroachments

No permanent private encroachment shall occur in the public right-of-way (ROW) or onto public property.

3.2.5. Street Vacations

The following conditions will apply to City vacation of streets and alleys due to lot consolidation.

1. The City shall approve analysis of the impacts on circulation patterns and shall determine whether the vacation will be detrimental.
2. A General Plan conformance analysis shall be completed.
3. State and local code requirements for processing and notifications shall be met.
4. The cost of relocating all utilities and other public improvements shall be borne by the developer.
5. Any public parking removed must be replaced either on-site or off-site through in-lieu fees. Such parking shall be in addition to required parking for the proposed use.
6. At the discretion of the City Council, all or portions of Main Street between Pacific Coast Highway and Orange Avenue may be vacated to be used for a pedestrian mall, subject to a public hearing. Prior to implementation, any on street parking lost shall be replaced.
7. Any development proposing the vacation of streets intersecting Pacific Coast Highway in District 1 shall provide a view corridor that meets the following criteria:
 - 1) Shall be located between Walnut Avenue and Pacific Coast Highway.
 - 2) Width shall be no less than the former right-of-way.
 - 3) No permanently installed solid structures greater than 42” in height shall be allowed within such view corridor.
 - 4) A 10’ wide pedestrian easement shall be provided through the development generally parallel to the vacated street.

3.2.6. Alleys and Vehicular Access Ways

The following standards shall apply to all vehicular access ways and alleys.

1. Access to development shall not be taken directly from Pacific Coast Highway, Main Street, or 5th Street; new driveway or alley curb cuts on these right-of-way are prohibited.

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3.2.6.1. Alley Vacation/Relocation

Alley vacations are allowed upon evaluation and necessary infrastructure studies required by the City.

1. The City shall approve analysis of the impacts on circulation patterns and shall determine whether or not the vacation will be detrimental.
2. A General Plan conformance analysis shall be completed.
3. State and local code requirements for processing and notifications shall be met.
4. The cost of relocating all utilities and other public improvements shall be borne by the developer.
5. An alley may be shifted or relocated upon the condition that any items (e.g. parking, service/loading areas, utilities and infrastructure) located within the alley are relocated along with vacating and dedicating the new alley.

3.2.6.2. District 1 Alley Special Paving

At areas where alleys intersect with paseos or parking structure entrances and/or exits, as shown in Figure 3-4, the alley paving shall be printed with distinctive patterns reminiscent of the wave pattern incorporated into the downtown sign program, as shown in Figure 3-5, to identify pedestrian links, entrances, and exits along the alleyway. The distinctive paving pattern will indicate to motorists that this is an area where motorists may find pedestrian activity.

3.2.7. Private Access Ways

Private access ways shall be developed pursuant to Chapter 231 of the HBZSO.

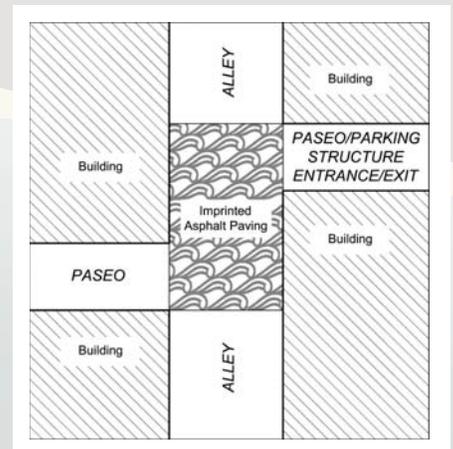


Figure 3-4 At areas where alleys intersect with paseos or parking structure entrances and/or exits, alley paving shall be printed with a distinctive wave pattern



Figure 3-5 The alley wave pattern mimics the pattern found in the downtown sign program

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3.2.21. Residential Buffers

These standards shall apply to the property line abutting the line depicted on Figure 3-9.

1. Minimum setbacks, as shown in Figure 3-10:
 - 1) 3' minimum side or rear setback.
 - 2) 25' maximum building height at the side or rear setback line.
 - 3) 45' maximum building height at 10' away from the side or rear property line.
 - 4) 55' maximum building height at 15' away from the side or rear property line, if such height is allowed by the provisions of the district in which the property is located.
2. The following activities associated with a commercial business are not permitted within 50' of the residential use:
 - Loading docks.
 - Service areas.
 - Noise- or odor-generating operations.
3. The following activities associated with a commercial business are not permitted within 20' of the residential use:
 - Trash and recycling enclosures.
 - Mechanical equipment.
4. Open space buffers incorporating landscaping shall be used to separate commercial and mixed uses from potentially incompatible uses. The width and treatment of the open space buffer will vary depending upon the types of potential land use conflicts to be resolved.



Figure 3-9 Locations in District 1 where residential buffers shall be applied

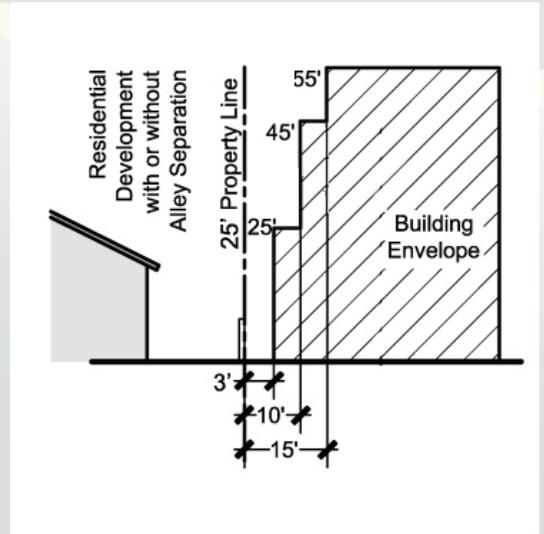


Figure 3-10 Graduated rear yard setbacks are required for parcels adjacent to single-family residential parcels or districts

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3.2.24.4. Parking for Outdoor Dining

- 1) Parking shall comply with Section 3.2.26. Parking of this Specific Plan.
- 2) No parking spaces shall be required for the outdoor dining portion of the restaurant if the outdoor dining area does not exceed:
 - Maximum of 20% of the restaurant area, not to exceed 400 square feet.
- 3) Any outdoor dining area that exceeds these standards shall provide 100% of the required parking for the area in excess of the above standard. Parking for the outdoor dining area shall be provided at a ratio of one (1) space per 125 sf of outdoor dining area.

3.2.24.5. Necessary Findings

In order to approve outdoor dining the Director or Zoning Administrator shall make the following findings:

- 1) The sidewalk's public use, pedestrian, transit and business services including but not limited to loading zones, bus stops, public phones, and benches, are not restricted.
- 2) Building entryways are not obstructed.
- 3) Pedestrian traffic volumes are not inhibited.
- 4) Handicapped accessibility is provided where required.

3.2.25. Outdoor Display Areas and Sales

Outdoor display areas, sales, and uses are encouraged within the Specific Plan Area provided that they enhance the pedestrian environment through the creative and appropriate use of outdoor spaces. The intent is to provide opportunities for businesses to display their products in a manner that enhances the public realm and creates an interesting, organized, and comfortable shopping and dining area. Creativity in the design of outdoor displays is encouraged and the quality, character, and design of all items placed outside should contribute in a positive way to the visual appearance of the community.

1. Temporary outdoor sales, whether on public or private property, shall be subject to approval of a Temporary Use Permit (TUP) by the Zoning Administrator as well as the following standards.
2. A TUP shall permit outdoor sales/display events subject to the following limitations:
 - 1) 5 years maximum per permit.
 - 2) 50 days maximum per calendar year.
 - 3) 10 consecutive days maximum.

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<u>Non-Residential Uses</u>		
<u>Use</u>	<u>District</u>	<u>Off-Street Parking Spaces</u>
Retail/Personal Services	District 1*	3 spaces per 1,000 sf
	All Other Districts	5 space per 1,000 sf
Personal Enrichment Services	District 1*	3 spaces per 1,000 sf
	All Other Districts	1 space per 35 sf of instruction area; or maximum 1 per 200 sf provided the number of students does not exceed required number of parking spaces, plus instruction area does not exceed 75% of floor area
Offices	District 1*	2 spaces per 1,000 sf
	All Other Districts	<ul style="list-style-type: none"> • Business/Professional: 4 spaces per 1,000 sf
Restaurant	District 1*	8 spaces per 1,000 sf Less than 12 seats: 3 spaces per 1000 sf
	All Other Districts	<ul style="list-style-type: none"> • Less than 12 seats: 5 spaces per 1,000 sf • More than 12 seats: 10 spaces per 1,000 sf
Hotel/Motel	All Districts	1.1 spaces per room, plus 1 space per passenger transport vehicle (minimum of 2 stalls) and 2 spaces for any manager’s unit and parking for other uses as required by this Section
Bed and Breakfast	District 1*	1.1 spaces per room
	All Other Districts	1 space per room, plus 1 guest space and 1 manager/owner space
Cultural Arts Facilities	All Districts	1 space per 300 sf
Assembly Uses	All Districts	Per HBZSO
Exceptions: a) At any time it deems necessary, the Planning Commission may require additional on-site parking to meet the parking demand generated by a use or development. b) No additional parking shall be required for new development in District 7. However, construction that proposes the removal of existing parking shall provide for the replacement of that parking at a 1:1 ratio. *Excluding Subdistricts		

Figure 3-15 Vehicle Spaces Required (1 of 2)

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<u>Bicycle Spaces Required</u>		
<u>Use</u>	<u>District</u>	<u>Off-Street Parking Spaces</u>
Non-residential Uses	District 1	1 bicycle space per 10 vehicle spaces required
	All Other Districts	<ul style="list-style-type: none"> • Buildings up to 50,000 square feet of gross building area: 1 bicycle space per 25 vehicle spaces required; minimum of 3 • Buildings over 50,000 square feet of gross building area: Director shall determine number of bicycle spaces based upon the type of use(s) and number of employees
Single-family	District 1	None required
	All Other Districts	None required
Multi-family	District 1	1 bicycle space per dwelling unit
	All Other Districts	1 bicycle space per 4 dwelling units

Figure 3-16 Bicycle Spaces Required

3.2.26.7. Tandem Parking

- 1) Up to 40% of the required on-site commercial parking may be provided as tandem parking with an attendant upon approval of a Conditional Use Permit by the Zoning Administrator.
- 2) Tandem parking configuration is allowed for residential uses.

3.2.26.8. Parking Screening

- 1) All off-street uncovered surface parking spaces shall be screened.
- 2) 32” high landscaping or landscaping combined with opaque material screen shall be provided, as measured from the adjacent parking surface.

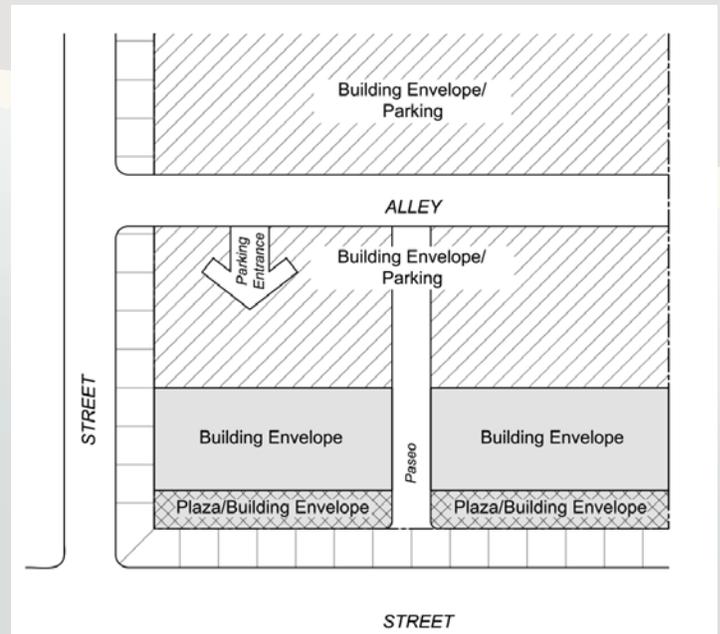


Figure 3-17 Access to parking lots and structures must be taken from an alley rather than a public street

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- 7) Parking structure facades over 150' in length shall incorporate at least 1 or more of the following:
 - a) Differentiation of the ground floor from upper floors.
 - b) Varying the arrangement, proportioning and/or design of garage floor openings at regular intervals.
 - c) Incorporating changes in architectural materials.
 - d) Projecting forward or recessing back portions or elements of the parking structure facade.
- 8) Facades of parking structures shall be designed without continuous horizontal parking floor openings. For example, openings shall be constructed to form a pattern of window-like openings.
- 9) Parking structure top floor wall designs must conform to 2 or more of the following options:
 - a) A top floor wall with an architectural focal point such as a glazed elevator and/or stair tower or top floor line trellis structure.
 - b) A top floor wall line articulated through a variation or step in cornice height or detail.
 - c) A top floor wall line with an articulated parapet cap.
- 10) Views into the upper floors of parking structures facing a public right-of-way shall be minimized through 1 or more of the following methods:
 - a) The use of planters integrated into the upper floors of parking structure facade design.
 - b) Decorative trellis work and/or screening as architectural elements on the parking structure upper floor facades.
 - c) Openings organized to reflect traditional building window patterns.
- 11) External elevator towers and stair wells shall maintain openings for public view.
- 12) Provisions shall be made to limit activation of individual car alarms within automated parking structures.
- 13) Where subterranean parking occurs, antennas shall be installed to ensure that emergency personnel can receive and transmit information from inside the structure.

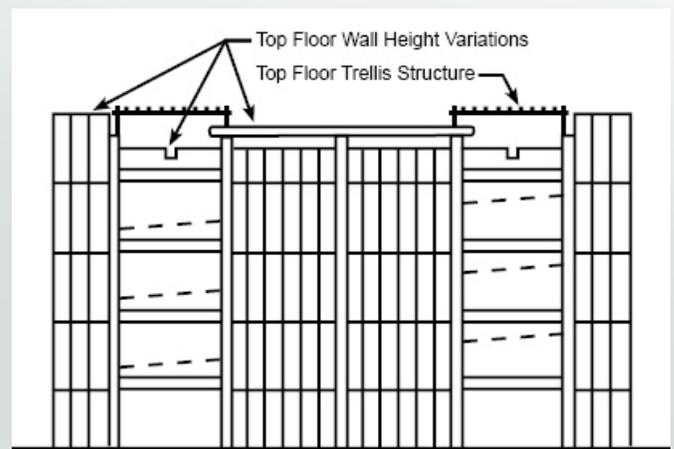


Figure 3-19 Parking structure top floor wall designs must be articulated with a variety of methods

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- 14) Developer/applicant shall coordinate installation with public safety departments and shall be responsible for installation and maintenance of such antenna.

3.2.26.11. District 1 Special Parking Standards

- 1) Parking for residential development shall be provided on-site.
- 2) Parking for hotel, motel, and bed and breakfast development shall be provided on-site.
- 3) Commercial development is permitted to satisfy some or all of the required parking off-site upon approval of a Conditional Use Permit by the Planning Commission. Any parking not provided on-site will require the payment of an in-lieu fee.
- 4) All new development will be required to replace any on-site parking associated with an existing use to be retained that is lost due to redevelopment as well as providing any net new parking required.
- 5) For intensification of an existing use, only all net new square footage shall provide parking as required by this Specific Plan. Existing square footage is grandfathered including tear down of building sf - parking associated with this square footage is not required to be parked at a higher number than what is existing.
- 6) All parking shall be provided underground on the Main Street Library property.
- 7) Vehicular access to parking lots and structures must be taken from an alley or side street.
- 8) In-lieu Fee
 - a) Participation in the parking in-lieu fee program shall require approval of a Conditional Use Permit from the Planning Commission.
 - b) The parking in-lieu fee amount shall reflect the market-rate value of constructing the additional parking space, which shall be established by the City Council.
 - c) The parking in-lieu fee may be paid in multiple installments. The first installment in an amount established by City Council Resolution for each parking space shall be paid prior to the issuance of building permits or a certificate of occupancy, whichever comes first. Any successive installments shall be paid and secured by a mechanism established in the conditions of approval.

9) Shared Parking Arrangements

Shared parking agreements may be developed if 2 or more land uses or businesses have distinctly different hours of operation or hours that do not substantially coincide or overlay with each other (e.g. theater vs. office). Parking required for a residential use shall not be included in a shared parking agreement.

When the above criteria are met, such uses may develop shared parking agreements to satisfy the parking requirements of this Specific Plan, approval of a Conditional Use Permit by the Planning Commission and in accordance with the following:

- a) Only 50% of the require parking may qualify for the shared arrangement.

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Any pervious pavement included in new development, as well as additional tree grates and special sub-surface construction recommended as part of the Specific Plan, will aid in minimization of runoff in the downtown core area. Additional curb inlets, dry wells, and potentially some underground percolation pipes may be required within projects to alleviate a storm water issue. Green roofs, urban bioswales, and rain gardens also minimize runoff and can be implemented by developers to reduce overall storm water generated on a site. Due to the close proximity to the Pacific Ocean, filtration of runoff water should be achieved whenever possible by increased soil contact prior to ocean discharge.

Any new water quality requirements resulting from future NPDES permits shall be incorporated into the project.

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3.3.1.3. Permitted Uses

- 1) Visitor-serving commercial uses are required at the ground floor street frontage in District 1, except for the following:
 - a) Within the Lake Avenue overlay, all uses permitted on the ground floor of District 1 are allowed. In addition, single-family residential, multi-family residential, and offices are allowed at the ground floor street frontage (see Figure 3-22).
 - b) Within Cultural Arts Subdistrict 1A - Cultural facilities are required at the ground floor street frontage (see Figures 3-22 and 3-36 for specific uses).
 - c) Within Neighborhood Subdistrict 1B - Permitted uses are limited to residential, office, personal enrichment, and personal service. (see Figures 3-22 and 3-38 for specific uses).
- 2) All permitted uses are allowed, but not required, behind the ground floor street frontage and above the ground floor in any portion of District 1 except Subdistrict 1A and 1B.



Figure 3-22 Locations in District 1 where residential and office uses are allowed and cultural facilities are required at the ground floor street frontage

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<u>Permitted Uses District 1</u>					
<u>Use</u>	<u>Permitted</u>	<u>Admin Permit</u>	<u>CUP from ZA</u>	<u>CUP from PC</u>	<u>TUP from ZA</u>
Accessory dwelling units		✓			
Artists' studios ¹	✓				
Banks and savings and loans branch offices ²	✓				
Carts and kiosks ³		✓			
Commercial parking			✓		
Cultural institutions	✓				
Eating and drinking establishments ⁴		✓			
Eating and drinking establishments, with less than 12 seats	✓				
Eating and drinking establishments, with alcohol			✓		
Eating and drinking establishments, with dancing				✓	
Eating and drinking establishments, with live entertainment				✓	
Food and beverage sales, without alcoholic beverage sales	✓				
Health and sports clubs				✓	
Home occupations ⁵		✓			
Hotels, motels, and bed and breakfasts				✓	
Live/work units ⁶		✓			
Offices, business and professional ⁷	✓				
Personal enrichment services ⁸		✓			
Personal services	✓				
Public facilities	✓				
Real estate businesses	✓				
Religious assembly			✓		
Multi-family housing, apartments, condominiums, and stock-cooperatives (up to 4 units)	✓				
Multi-family housing, apartments, condominiums, and stock-cooperatives (more than 4 units)			✓		
Single-family detached dwellings ⁹	✓				
Retail markets with alcohol sales ¹⁰				✓	
Retail sales	✓				

Figure 3-24 Permitted Uses District 1 (1 of 2)

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<u>Summary of Development Standards District 1</u>			
	<u>All District 1</u>	<u>Lake Ave.</u>	<u>Section</u>
Allowable Uses	See Figure 3-24	See Figure 3-24 plus Residential/ Office on ground floor	
Minimum Parcel Size	25' street frontage & 2,500 sf net area	25' street frontage & 2,500 sf net area	3.3.1.5
Maximum Site Coverage	None required	None required	3.3.1.6
Maximum Density	<25,000 sf net site area: 30 du/ac > 25,000 sf net site area: 50 du/ac	30 du/ac	3.3.1.7
Minimum Building Height	25'	25'	3.3.1.8
Maximum Building Height	<ul style="list-style-type: none"> • < 25,000 sf net site area: 45' & 4 stories • ≥ 25,000 sf net site area: 55' & 5 stories 	35' & 3 stories	3.3.1.8
Upper Story Setback (4th-5th story)	10' average	None required	3.3.1.9
Front Yard Setback	0'- Max. 5'	Min 10'	3.3.1.10
Interior Side Yard Setback	0'	3'	3.3.1.11
Exterior Side Yard Setback	Equal to front setback	5'	3.3.1.11
Corner Setback	25'	None required	3.3.1.12
Rear Yard Setback	3'	3'	3.3.1.13
Public Open Space	3% to 5%	None required	3.3.1.14
Storefront	Refer to Section	None required	3.3.1.15
Public Art	Refer to Section	None required	3.3.1.16
Paseos	Min 1/250' street frontage	None required	3.3.1.17
Loading and Service Areas	Refer to Section	None required	3.3.1.18
Refuse and Recycling Enclosures	Refer to Section	None required	3.3.1.19
Cultural Arts Subdistrict 1A	Refer to Section	None required	3.3.1.20

Figure 3-25 Summary of Development Standards District 1

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3 - LAND USES & DEVELOPMENT STANDARDS

3.3.1.4. Summary of Development Standards

Figure 3-25 presents a summary of the development standards within District 1. This figure is only a summary of the standards and the sections of the plan referenced in the figure must be consulted for the complete requirements of each standard.

3.3.1.5. Minimum Parcel Size

25' of frontage and 2,500 square feet of net site area.

3.3.1.6. Maximum Site Coverage

No maximum site coverage shall be required.

3.3.1.7. Maximum Density

60 dwelling units per net acre.

3.3.1.8. Building Height

- 1) 25' minimum.
- 2) 45' and 4 stories maximum for developments with less than 25,000 sf net site area.
- 3) 55' and 5 stories maximum if the overall net site area is 25,000 sf or greater.

3.3.1.9. Upper Story Setback

10' average setback from the ground floor facade for portions of the front facade on the fourth and fifth stories.

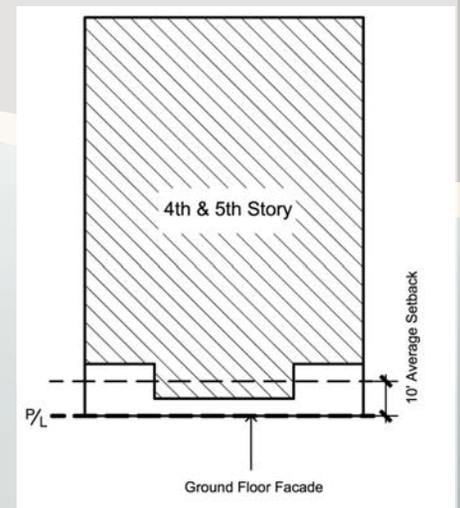


Figure 3-26 For the fourth and fifth stories, portions of the front facade shall be setback an average of 10 feet from the ground floor facade

3.3.1.10. Front Yard Setback

- 1) 40’ minimum dedication from the center line of the roadway for parcels fronting Main Street.
- 2) 15’ minimum dedication of sidewalk area from edge to the property line for parcels fronting Pacific Coast Highway between 1st Street and 6th Street.
- 3) 5’ maximum setback from the ultimate property line for commercial and mixed-use development, except where outdoor dining, open-air commercial facilities, or plazas are provided.
- 4) 10’ minimum setback for exclusively residential development. Setback may be reduced to 5’ on up to 50% of the frontage provided that the average setback for the total street frontage is 10’.
- 5) A 10’ minimum landscaped setback shall be provided where a parking lot is adjacent to a public right-of-way.

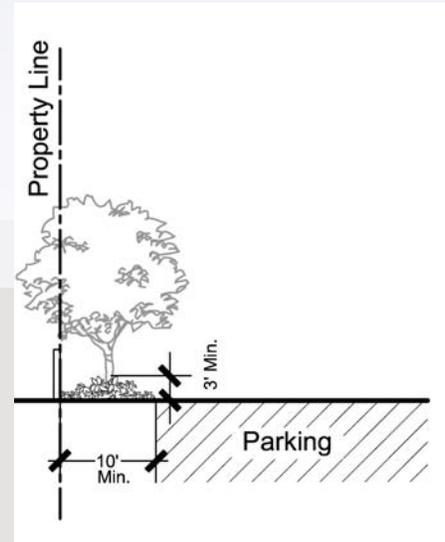


Figure 3-27 Parking lots shall be setback a minimum of ten feet from the public right-of-way.

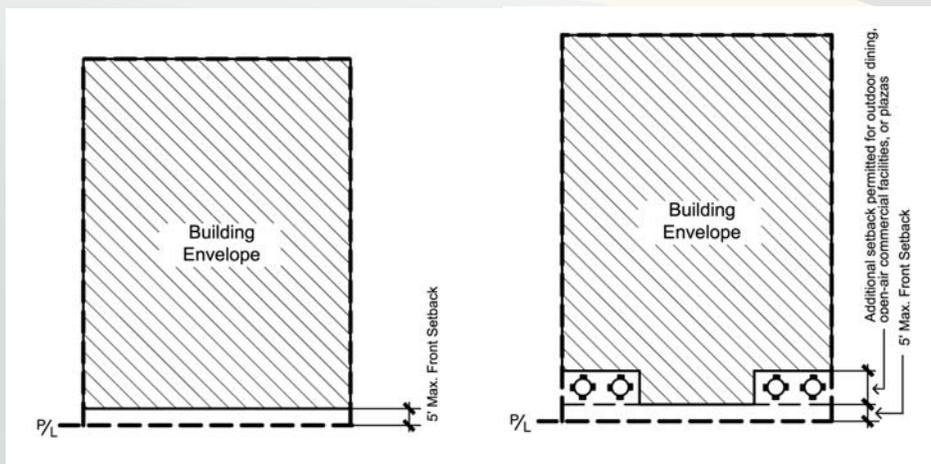


Figure 3-28 For commercial development, a maximum five-foot setback shall be required, except where outdoor dining, open-air commercial facilities, or plazas are provided

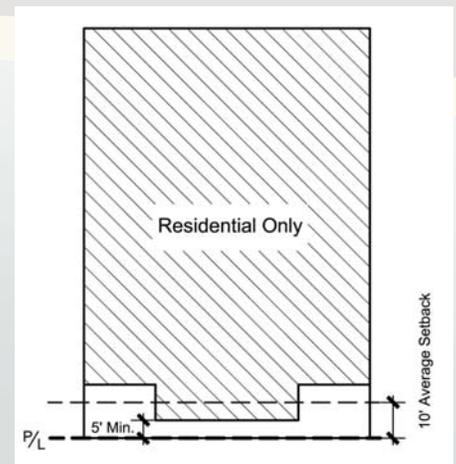


Figure 3-29 On parcels where residential only is permitted, the average setback for the total street frontage is ten feet

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3.3.1.21. Neighborhood Subdistrict 1B

1) Purpose

The purpose of the Neighborhood Subdistrict 1B is intended to provide a transition zone between the existing residential areas and the commercial Main Street Corridor. Consequently, mixes of office and residential uses are permitted.

2) Boundary

From the alley between 3rd Street and 2nd Street to 1st Street, between Walnut Avenue and Orange Avenue.

3) Permitted Uses

- a) Residential, Office, and limited commercial uses shall be permitted on the ground floor within the District 1 Neighborhood Subdistrict 1B.
- b) Figure 3-38 presents uses permitted within District 1 Neighborhood Subdistrict 1B. The table details whether the use is permitted or whether the use requires a Conditional Use Permit from the Zoning Administrator, or a Conditional Use Permit from the Planning Commission.
- c) Other office-residential related uses not specified herein may be allowed subject to the approval of the Director.

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<u>Permitted Uses Neighborhood Subdistrict 1B</u>		
<u>Use</u>	<u>Permitted</u>	<u>CUP from ZA</u>
Eating and drinking establishments, with less than 12 seats	✓	
Home Occupations ¹	✓	
Live / Work Units ²	✓	
Mixed Residential / Office Use	✓	
Personal Enrichment Services ³	✓	
Personal Services	✓	
Multi-family housing, apartments, condominiums, and stock-cooperatives (up to 4 units)	✓	
Multi-family housing, apartments, condominiums, and stock-cooperatives (more than 4 units)		✓
Single-family detached dwellings ⁴	✓	
Office	✓	
¹ Pursuant to Section 230.12 of the HBZSO ² Provided that: <ul style="list-style-type: none"> • The live/work unit is permitted to be a maximum of 3,000 square feet. • The non-residential area is permitted to be a maximum 50% of the area of each live/work unit. • The non-residential area function shall be limited to the first or main floor only of the live-work unit. • A maximum of 5 non-residential worker or employees are allowed to occupy the non-residential area at any one time. • Dwelling units that include an office that is less than ten percent of the area of the dwelling unit shall not be classified as a live/work unit. ³ Not to exceed 5,000 square feet ⁴ That comply with the standards provided in Section 3.3.4. District 4 - Established Residential and the standards contained in the Residential Infill Lot Developments Ordinance in Section 230.22 of the HBZSO		

Figure 3-38 Permitted Uses Neighborhood Subdistrict 1B of District 1

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3.3.2.1. Purpose

This district extends southeast of the downtown core adjacent to Pacific Coast Highway. The principal purpose of this district is to provide commercial facilities to serve seasonal visitors to the beaches as well as to serve local residents on a year round basis. This district also provides a continuous commercial link between the downtown and the visitor-commercial/recreation district near Beach Boulevard. The area of District 2 represents the visitor-serving commercial portion of the approved Pacific City project. Pacific City is a 31-acre mixed use project consisting of 516 residential units and seven commercial buildings with retail, office, restaurant, cultural and entertainment uses. The commercial portion of Pacific City is also planned to have carts, kiosks, outdoor dining, live entertainment indoors and outdoors and a boutique hotel. The residential component of Pacific City is described in District 5. The approved development described above is consistent with the provisions stated for this district.

3.3.2.2. Boundaries

District 2 boundaries are shown on Figure 3-40. District 2 extends from First Street to Huntington Street between Pacific Coast Highway and Pacific View Avenue.

3.3.2.3. Permitted Uses

- 1) Visitor-serving commercial uses must be a part of all development proposals in this district, with a minimum requirement that the entire street level be devoted to visitor-serving commercial uses. Any alterations or additions shall be consistent with the approved plans for the Pacific City development and shall comply with Section 2.5.10 of this Specific Plan. All new construction shall require approval of a Conditional Use Permit from the Planning Commission.
- 2) Figure 3-41 presents uses permitted within District 2. The table details whether the use is permitted or whether the use requires an Administrative Permit, a Conditional Use Permit from the Zoning Administrator, or a Conditional Use Permit from the Planning Commission. For permitted uses, other visitor-serving related uses as described in the Coastal Element of the General Plan that have the same parking demand as the existing use not specified herein may be allowed subject to the approval of the Director.

3.3.2.4. Summary of Development Standards

Figure 3-42 presents a summary of the development standards within District 2. This figure is only a summary of the standards and the sections of the plan referenced in the figure must be consulted for the complete requirements of each standard.

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b) Parcels with greater than 100' of frontage:

- 20% of lot width:
 - Interior: 7' minimum
 - Exterior: 15' minimum

3.3.4.12. Rear Yard Setback

Minimum rear yard setback requirements:

- 1) 3' minimum setback for parcels fronting on Pacific Coast Highway.
- 2) 7.5' minimum setback for parcels fronting all other streets.
- 3) Upper stories of structures may be cantilevered into the rear setback; however, a setback of 2.5' minimum from the rear property line shall be maintained.

3.3.4.13. Garage Setback

A 5' setback is required for garages with alley access in order to meet the required 25' turning radius from the garage to the opposite side of the alley.

3.3.4.14. Common and Public Open Space

- 1) Projects that maintain the 25' front setback along Pacific Coast Highway shall be allowed to use the front setback area towards common open space as required in Section 3.2.16. Open Space. Any encroachments into the 25-foot front setback area shall require common open space to be located behind the front setback.
- 2) No public open space shall be required.

3.3.4.15. Street Frontage

- 1) Single-family dwelling units not fronting Pacific Coast Highway shall have a front porch element that faces onto the primary street frontage. The front porch shall be allowed to encroach 5' into the front setback area.

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3.3.5.1. Purpose

This district is intended to allow high density residential uses. New residential development will provide a population base to help support the commercial and office uses in the downtown area. District 5 represents the Pacific City residential component and the Waterfront Residential development. The Pacific City residential component is approved for 516 condominium units and outdoor recreational amenities including a 2-acre Village Green park. The Waterfront Residential development was constructed in 2004 and consists of 184-residential units located north of the waterfront hotels in District 3. The approved and existing developments described above are consistent with the provisions stated for this district.

3.3.5.2. Boundaries

District 5 boundaries are shown on Figure 3-54. District 5 includes two consolidated parcels. One parcel is bounded on the north by Atlanta Avenue, on the east by Huntington Street, on the south by Pacific View Avenue, and on the west by First Street. The second parcel includes the area north of Pacific View Avenue between the prolongation of Sunrise Drive and Beach Boulevard.

3.3.5.3. Permitted Uses

Figure 3-55 presents uses permitted with a Conditional Use Permit from Planning Commission within District 5. Any modifications or additions to individual residential units shall be permitted upon review by the Department of Planning for compliance with all applicable provisions of this Specific Plan in addition to applicable requirements, conditions of approval and development plans for the approved project.

<u>Permitted Uses District 5</u>	
<u>Use</u>	<u>CUP from PC</u>
Multi-family housing, condominiums, stock-cooperatives or apartments ¹	✓
Public transportation center	✓
¹ Must be permanently attached	

Figure 3-55 Permitted Uses District 5

3.3.5.4. Summary of Development Standards

Figure 3-56 presents a summary of the development standards within District 5. This figure is only a summary of the standards and the sections of the plan referenced in the figure must be consulted for the complete requirements of each standard.

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3.3.7.1. Purpose

This district is intended to preserve and protect the sandy beach area within the Specific Plan Area boundaries while allowing parking and auxiliary beach-related commercial and convenience uses. The beach will also be used for special events throughout the year, such as the U.S. Open of Surfing, the AVP Pro Beach Volleyball tournament, the Shoreline Marathon, the NPPL Paintball Tournament, and the Duck-a-Thon. The beach parking and plaza areas north of the pier are also used weekly for a Farmers' Market and craft fair. Approximately half of the beach frontage in the district is City beach; the remainder of the beach frontage is owned by the State of California.

3.3.7.2. Boundaries

District 7 boundaries are shown on Figure 3-61. District 7 is bounded by Pacific Coast Highway on one side and the Pacific Ocean on the other side. The district extends from Goldenwest Street to Beach Boulevard, except for the area which is part of District 6.

3.3.7.3. Permitted Uses

Figure 3-65 presents beach open space uses and public facilities, as well as other related uses or public facilities not specified herein, that may be allowed subject to approval of the Design Review Board. Any change of such use or public facilities shall be subject to the approval of the Director.

3.3.7.4. Prohibited Uses

Storage containers and stand-alone storage buildings shall be prohibited in beach areas.

3.3.7.5. Tiered Parking

Enhancement of existing parking capacity located north of the pier between PCH and the beach is intended to enhance the pedestrian connectivity and experience along the west side of PCH and provide additional "tuck-under" parking opportunities. An expansion of the existing landscaped parkway reflecting similar design concepts and amenities found north of this area could be created by covering the existing upper tier of parking. In addition, "tuck-under" parking opportunities may exist by excavating versus expansion of the existing tiered parking height.

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The precise design of additional “tuck-under” parking must adhere to the following standards:

- a) Tiered parking shall be limited to the area identified in Figure 3-62.
- b) Parking shall not result in the loss of recreational sand area.
- c) The top of parking structures shall be at the same elevation of the Pacific Coast Highway sidewalk or located a minimum of one-foot below the maximum height of the adjacent bluff.
- d) Views from and toward PCH shall be maintained.
- e) Additional “tuck-under” parking provided shall be located as shown in Figure 3-62.



Figure 3-62 Tiered parking in District 7 shall be limited to the highlighted areas

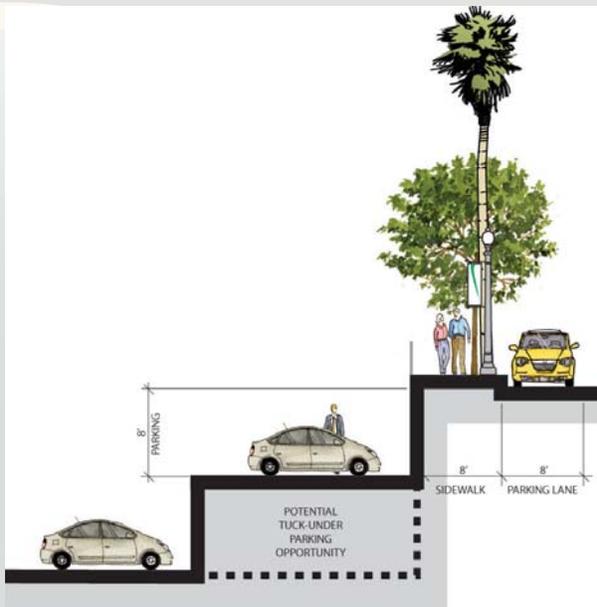


Figure 3-63 Existing Condition: Tiered parking is permitted in District 7

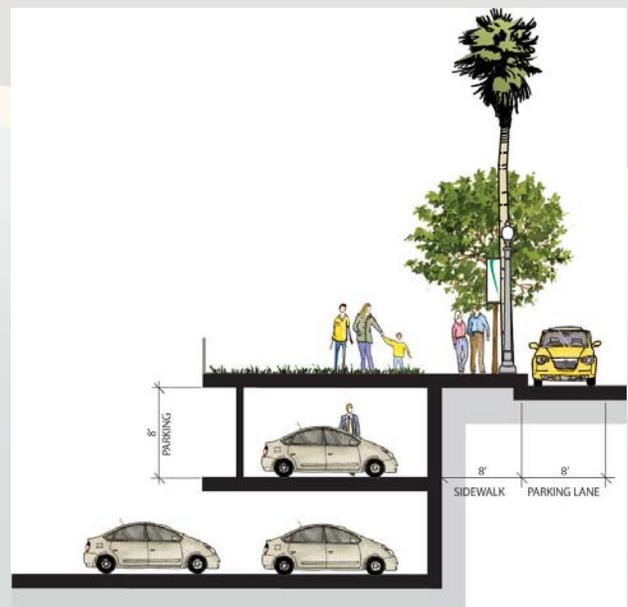


Figure 3-64 Potential “tuck-under” style parking. Tiered parking is permitted in District 7

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<u>Permitted Uses District 7</u>		
<u>Use</u>	<u>CUP from PC</u>	<u>Design Review Board</u>
Access facilities		✓
Basketball courts		✓
Beach concession stands ¹		✓
Beach maintenance and storage facilities ²		✓
Fire rings		✓
Paddle board courts		✓
Parking lots		✓
Park offices and playground equipment		✓
Public restrooms		✓
Public transit facilities and associated structures, dressing rooms or showers ³		✓
Tiered Parking Structure	✓	✓
Trails, bicycle or jogging, and support facilities		✓
Volleyball net supports		✓
¹ Shall be limited to 2,500 square feet and spaced at intervals no closer than 1,000 feet. Beach concession structures shall be located within or immediately adjacent to paved parking or access areas. ² Must be enclosed within a building, if that building is also utilized for purposes other than maintenance ³ May only be constructed within the existing paved parking areas or in areas that are not part of the beach		

Figure 3-65 Summary of Permitted Uses District 7

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Summary of Development Standards District 7		
	All District 7	Section
Minimum Parcel Size	None required	3.3.7.6
Maximum Site Coverage	None required	3.3.7.7
Maximum Density	None required	3.3.7.8
Maximum FAR	None required	3.3.7.8
Maximum Building Height	20'	3.3.7.9
Upper Story Setback	None required	3.3.7.10
Front Yard Setback	None required	3.3.7.11
Side Yard Setback	None required	3.3.7.12
Rear Yard Setback	None required	3.3.7.13
Public Open Space		3.3.7.14

Figure 3-66 Summary of Development Standards District 7

3.3.7.6. Summary of Development Standards

Figure 3-66 presents a summary of the development standards within District 7. This figure is only a summary of the standards and the sections of the plan referenced in the figure must be consulted for the complete requirements of each standard.

3.3.7.7. Minimum Parcel Size

No minimum parcel size shall be required.

3.3.7.8. Maximum Site Coverage

No maximum site coverage shall be required.

3.3.7.9. Maximum Density/FAR

No maximum density or FAR requirement shall be applied in this District.

3.3.7.10. Maximum Building Height

20' maximum building height. No maximum building height shall be required for lifeguard towers or other facilities necessary for public safety. No parking surface or structure shall exceed the adjacent elevation of Pacific Coast Highway.

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3.3.7.11. Upper Story Setback

No minimum upper story setback shall be required.

3.3.7.12. Front Yard Setback

No minimum front yard setback shall be required.

3.3.7.13. Side Yard Setback

No minimum side yard setback shall be required.

3.3.7.14. Rear Yard Setback

No minimum rear yard setback shall be required.

3.3.7.15. Public Open Space

Public open space and pedestrian access shall be major considerations of development in this District.

3.3.8 Resource Production Overlay

Purpose. The Downtown Specific Plan area overlies long-productive oil pools. Many facilities are still operating because of the extent of the remaining reserves; therefore oil production will continue to be permitted in parts of this area.

The City provides for oil facilities by designating oil “suffix” zoning Districts in connection with an underlying base zone such as a commercial or residential District. Both oil facilities allowed by suffix and the other uses allowed by the base zone are permitted. Currently, the City has two oil suffixes the “)” which allows existing oil wells and attendant facilities but no new wells and the “o1” which allows the drilling of new wells in addition to all uses in the “o” District. These suffixes, with certain modifications, are also employed in this Specific Plan. In addition to the oil suffixes three Resource Production Overlays have been identified. Existing and/or expanded oil production may continue in these areas provided that the additional conditions outlined in this subsection are met.

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3.3.8.1 Oil Overlay “A”

The regulations in this overlay District facilitate continued oil recovery, but require all new facilities to be concentrated into a second, soundproofed and landscaped expansion of the existing oil site and encourage the expeditious removal of existing wells from oil overlay “B”.

Boundaries. Oil overlay “A” includes an existing oil island located in District #2, between 19th and 18th streets from Pacific Coast Highway to the area is Walnut Avenue.

Regulations. New wells and related facilities shall be permitted in accordance with the -01 suffix and related provisions in the Huntington Beach Ordinance Code provided, however, that the following additional conditions are met:

- a) Any new well must be part of a secondary or other enhanced oil recovery project or used as replacement of an existing well.
- b) A schedule for abandonment of all wells operated by the project proponent which are located within Bolsa Chica State Beach shall be submitted to and approved by the Director of Development Services prior to the drilling of any new well. This schedule may be amended at the discretion of the Director of Development Services.
- c) The project proponent shall agree to a memorandum of understanding with the City as a condition for approval, stating that no new wells shall be drilled by that company on Bolsa Chica State Beach (oil overlay “B”) nor shall the existing wells be redrilled except, in such cases where: 1) the redrilled well will be produced by a “subsurface” or “down-hole” pump, only, or 2) the redrilled well will be produced by other new technology with fewer visual and environmental impacts than a conventional ball and plunger pump, or 3) an intensified screening plan is approved by the Director of Development Services which substantially improves the appearance of the area.
- d) The operation site shall be screened by a wall, fence, or structure in keeping with the character of the area. The site also shall be landscaped so as to ensure visual compatibility with the surrounding area. A screening and landscaping plan must be submitted and approved by the Director. All structures shall generally conform to the height limits and setback requirements of the base District. The Director may waive these restrictions if it would result in better overall soundproofing, odor reduction and/or visual compatibility.

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3.3.8.2 Oil Overlay “B”

The regulations in this overlay facilitate continued oil recovery, wells may be redrilled if surface pumping units are replaced with subsurface ones. Drilling of new wells may be permitted but only if the result is a significant reduction in the amount of space used for oil operations on the beach.

Boundaries. Oil overlay “B” comprises a section of Bolsa Chica State Beach currently in oil production in District #11 between Goldenwest and 11th streets.

Regulations. Wells may be redrilled in accordance with the -o suffix in the Huntington Beach Ordinance Code provided, however, that the following additional conditions are met:

The operator submits a report to the Department of Development Services explaining why there is no other feasible, environmentally less damaging inland site (such a report must be approved by the Director); or agrees to a memorandum of understanding with the City stating that the redrilled well will be produced by a subsurface or down-hole pump or other new technology with fewer visual and environmental impacts than a conventional ball and plunger pump.

A schedule for abandonment of all wells operated by the project proponent which are located within Bolsa Chica State Beach shall be submitted to and approved by the Director of Development Services prior to the drilling of any new wells. This schedule may be amended at the discretion of the Director of Development Services.

All redrilling operations shall be limited to a period from October 1 to May 31, except for emergencies for which the Fire Chief may waive these seasonal restrictions, but shall require soundproofing in accordance with Title 15 of the Huntington Beach Municipal Code.

New wells may be permitted if they are part of an overall consolidation plan which significantly reduces the area used for oil facilities or expedited the removal of existing oil facilities within the overlay area. A consolidation plan must be submitted to the Director of Development Services for approval before a permit for drilling any new well will be issued. All drilling operations must be conducted in accordance with the requirements of the o1 suffix in the Huntington Beach Ordinance Code.

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3.3.8.3 Oil Overlay “C”

The regulations in the overlay facilitate continued oil recovery and provides for future oil production needs.

Boundaries. Oil overlay “C” is an irregularly shaped site in District #8A between Lake Street and Huntington Avenue and Atlantic Avenue.

Regulations. Well drilling and redrilling shall be permitted in accordance with Title 15 of the Huntington Beach Municipal Code and with the 0 and 01 suffix and related provisions in the Huntington Beach Ordinance Code. A conceptual site plan for the entire overlay area must be submitted prior to permitting any project development or subdivision of land within the overlay. The plan shall include at least one (1) oil island of not less than two (2) acres in size for new oil well drilling and oil production. Such island(s) shall be incorporated into the overall development plan so that noise, odor and visual impacts on the residences are minimized, and safe access to the oil site(s) is provided. Findings that at least one such island so designed is incorporated into the plan shall be made by the Planning Commission before approving any development project.

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Chapter 4 - Design Guidelines

4.1. Introduction

The goal of this chapter is to provide clear and useful recommendations for the design, construction, review, and approval of development in Downtown Huntington Beach. The guidelines are intended as a reference point for a common understanding of the minimum qualitative design expectations for the downtown. The guidelines are offered as one way of achieving attractive and functional projects that compare favorably with established community standards. All development shall comply with the spirit and intent of the design guidelines presented in this chapter.

Designers and developers are urged to become familiar with these guidelines and to apply the guidelines to the design of projects from the very beginning to assure that the design, review, and permitting processes are as efficient as possible. **Designers and developers are also urged to recognize that these guidelines are a minimum starting point for quality development.** No claim can be made that these guidelines encompass every possible technique for achieving a high level of design quality. The designer is encouraged to use his or her own creativity and experience to improve upon the means for realizing this highest level of quality design. However, it is of utmost importance that project designers incorporate ocean inspired elements into their streetscape and hardscape designs. Incorporating an ocean theme into projects will unify the downtown and create a visual reminder of the City's "Surf City" culture and create an iconic downtown environment.

The design guidelines may be interpreted with some flexibility in the application to specific projects, as not all design criteria may be appropriate for each project. In some circumstances, one guideline may be relaxed in order to accomplish another, more important, guideline. The overall objective is to ensure that the intent and spirit of the design guidelines are followed and to attain the best possible design within reason.

Following are design guidelines for commercial, single-family, and multi-family development within the Specific Plan Area. There are also additional guidelines for special design considerations, such as mixed-use projects, corporate architecture, hotels, and public open space.

Design guidelines that promote sustainable design features are marked with a leaf symbol. 

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- 11) Drainage should be directed to permeable areas such as yards, open channels, or vegetated areas, avoiding discharge to roads and minimizing discharge to the storm drain system.
- 12) Common box mailbox enclosures should be designed similar in form, materials, and color to the surrounding buildings.

4.4.4.2. Trash and Recycling Enclosures

Trash and recycling enclosures should be carefully designed, located, and integrated into the site plan.

- 1) Every property should provide a trash and recycling enclosure that is capable of handling the refuse generated by that site.
- 2) At least half of the trash and recycling area should be dedicated to recycling containers.
- 3) The Huntington Beach Business Improvement District should help to facilitate coordination between Downtown business owners and commercial trash service providers to help ensure placement and size of bins and level of service provides for a safe and clean environment downtown.
- 4) Trash and recycling enclosures should be screened with landscaping and wall materials that are architecturally compatible to the building design.
- 5) The trash and recycling enclosure should be consistent with the design of the project and the building architecture. Similar or the same materials should be used on the enclosure as the surrounding buildings. Architecturally designed roof structures should be used to create a finished looking structure.
- 6) A pedestrian entrance to the trash and recycling enclosure should be provided so that the large access gates do not have to be opened as often.
- 7) Trash and recycling bins should be conveniently accessible for collection and maintenance and should not block access drives during loading operations.
- 8) Trash and recycling enclosures should be separated from adjacent parking stalls by a minimum three-foot wide planter with low-growing plant materials to ensure that adequate space is available for passengers to access a vehicle in an adjacent parking space.
- 9) Drainage from adjoining roof and pavement should be diverted around the trash and recycling area.



Trash and recycling enclosures should be carefully integrated into the site plan

Chapter 5 - Circulation & Parking

5.1. Introduction

Achieving the Specific Plan goals and objectives will require improvements to the existing transportation system. Improvements will include construction of wider sidewalks that provide increased capacity and ADA paths of travel from streets to building entrances, realignment of selected roadways, expansion of bicycle and transit facilities, strategies to better manage the parking demand and increase the parking supply. The improvements are intended to make it easier for residents, employees, and visitors to bicycle, walk, and utilize transit to get to and from downtown.

This chapter includes a summary analysis based upon detailed technical reports completed in coordination with the Specific Plan and summarized improvement recommendations from those reports.

5.2. Existing Street Network Characteristics

The streets within the Specific Plan Area make up a majority of the publicly-owned property, providing for vehicular, bicycle, and pedestrian access. The Downtown circulation system generally consists of a grid system of two-lane streets running parallel and perpendicular to Pacific Coast Highway. Exceptions to this occur at the north end of Downtown, where the Main Street alignment turns north approximately 45 degrees, and the adjacent streets are oriented accordingly.

A system of public alleys between the streets provide delivery and loading access for businesses, access to private parking areas and access to residential garages and carports. Some businesses also have a rear entrance to accommodate customer access from the alley. The existing street network is described below.

5.2.1. Main Street

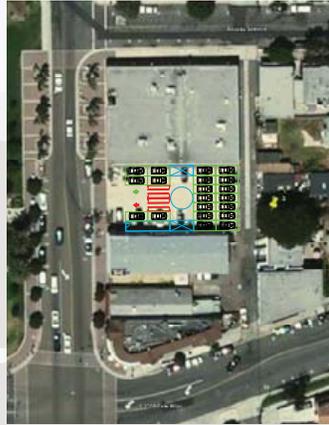
Main Street is a two-lane roadway designated as a local street and is the “main” street downtown. It carries moderate levels of vehicular traffic and has significant pedestrian levels. Main Street experiences vehicular congestion during the peak summer season and on event days due to increased parking, bicycle and pedestrian activity. Metered parking is provided between Pacific Coast Highway and Palm Avenue.

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demand times. Businesses could also enter into agreements with each other, with those with more spaces than needed being compensated in some way by businesses needing spaces for their employees. The new lots constructed on vacant parcels could also serve employees only.

5.6.3.8. Parking Structures

Additional new conventional or automated parking structures will be needed within the downtown to accommodate the future parking demand. An underground parking structure is proposed as part of the Cultural Arts Overlay area and other structures could be provided on City or privately owned lots. One optional solution would be to provide an automated structure on the site adjacent to the Huntington Beach Art Center, which would provide approximately 200 new parking spaces.



An automated parking structure adjacent to the Art Center would provide approximately 200 new parking spaces - Images compliments of Apex Parking

5.6.3.9. Tiered Beach Parking

Additional parking could be provided in an automated lot or as “tuck-under” parking on the seaward side of Pacific Coast Highway in the existing beach parking lots. The area would need to be graded and retaining walls would need to be installed to retain views from downtown. All parking would be located below the adjacent height of Pacific Coast Highway. Refer to Section 3.3.7.5.

5.6.3.10. Parking Wayfinding Signage

A series of static and electronic parking wayfinding signs should be installed throughout the downtown. The signs will indicate the location of parking facilities and the number of spaces available at each designated location. The signs will direct users to harder-to-find spaces, manage the parking supply more efficiently, and minimize vehicle circulation. These information and direction signs should be located on the approaches to Downtown to inform the driver of where parking spaces are available, so that drivers can make the appropriate driving decisions prior to reaching a parking structure that is already full. The signs should also be posted near the major parking facilities, indicating how much parking is available at that facility, and directing drivers to other locations where parking is available, if necessary.

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have a total pumping capacity of 25,050 gallons per minute (gpm) and could normally supply 21,400 gpm if the City operated all of the wells at 100 percent throughout the year.

Imported water is supplied to the City through three imported service connections known as the OC-9, OC-35 and OC-44. The combined capacity of the service connections is 22,000 gpm.

Per historical water data, in 2005 the City pumped approximately 14,945 acre feet per year (afy) of groundwater and purchased 17,847 afy of imported water. Water projections indicate an increase in both imported and groundwater supplies by 2010. However, recent statewide initiatives could potentially reduce the imported water supply as a result of interim pumping restrictions on the State Water Project that could reduce supply by up to 30 percent in the near future. It is through aggressive water conservation and efficient water use that will continue to allow the City to provide reliable water service to its customers.

7.2.1.2. Water Demand

The 2005 Urban Water Management Plan (UWMP) projects that in 2010, under a normal year, the City of Huntington Beach will use approximately 35,000 afy of water and during multiple dry years the City could consume as much as 37,000 afy. In order to maintain and ensure water reliability, it is necessary that the City's available water supply satisfies the total water demand of the Specific Plan Area under multiple dry year conditions. Based on preliminary analysis, the proposed increase in density within the Specific Plan Area will increase the total City water demand specified in the 2005 UWMP by approximately one percent. Recently adopted conservation measures influence the water demand factors used to determine water demand for the Specific Plan Area. Implementation of aggressive conservation measures will increase the water supply reliability.

The water supply availability for each project will be analyzed based on the water surplus identified in the most recently adopted UWMP. The water supply analysis has been analyzed under the Water Supply Assessment and Wet Utilities Study. In addition, the City shall require each development to prepare an equivalent of a Water Supply Assessment that is not otherwise subject to Senate Bills 610 and 221 (or at least those developments that cross a certain development threshold) to ensure that adequate water supplies can be provided as each development proceeds.

7.2.1.3. Water Conservation

In an effort to comply with statewide water goals and reduce water usage by 20 percent in per capita water use statewide by the year 2020, the City is recommending conditions of approval that could reduce residential demand by as much as 35 percent. Recommended conditions of approval for projects within the Specific Plan Area should aid in reducing the overall water usage.

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Conversion of existing buildings and public areas to comply with the same conservation measures can further assist in meeting the 20 percent reduction goal. The following water saving technologies can be implemented on a project basis to comply with these requirements.

- Waterless or lower flush urinals should be specified in all public areas, including restaurant and commercial bathrooms.
- Low-flush toilets should be installed in all new residential units and encouraged through rebates or other incentives in existing homes.
- Low-flow shower heads and water faucets should be required in all new residential and commercial spaces and encouraged in existing developed properties.
- Water efficient kitchen and laundry room appliances should be encourage through rebates for both residential and commercial units.
- Landscaping should be completed with drought tolerant plants and native species.
- Irrigation plans should specify the use of smart controllers, have separate irrigation meters, and follow the City of Huntington Beach “Water Efficient Landscape Requirements”.
- Water features should be designed with consideration to conserve water.

7.2.1.4. Water Distribution

The existing water infrastructure serving the Specific Plan Area consists of a 20-inch transmission supply loop in Lake Street that continues into 3rd Street at Orange Avenue and then runs northwest along Olive Avenue to Goldenwest Street. There is also an existing 18-inch line in Olive Avenue running southeast from Lake Street to 1st Street and then southwest down 1st Street to Pacific View Avenue where it again turns southeast and continues down Pacific View Avenue to Beach Boulevard. There are 12-inch and 8-inch distribution lines in Pacific Coast Highway and 8-inch lines in most streets running perpendicular to Pacific Coast Highway. Some existing lines running in the alleyways and streets within the Specific Plan Area are currently 6 inches in diameter.

The additional demand produced by each development within the Specific Plan Area, along with the typical fire flow required by the Huntington Beach Fire Department for this type of proposed land use, will be used to adequately size the pipelines within the Specific Plan Area. Additional hydraulic water studies will be required as individual development occurs to verify the pipeline diameter required to adequately support each specific project.

Pacific Coast Highway and Beach Boulevard are Caltrans right-of-ways and will require dual water lines, with one pipeline in each side of the roadway. This arrangement would minimize impacts to traffic flow by avoiding the need to cross these major arterials to construct and

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maintain water services to each development. The minimum pipeline diameter required is 12 inches. All water facilities, including the number and placement of meters and location of backflow protection devices shall satisfy the latest Department of Public Works Standards and Policies and require Department of Public Works approval.

7.2.2. Wastewater System Conditions

7.2.2.1. Wastewater Treatment

The Orange County Sanitation District (OCSD) provides wastewater collection and treatment services for the City of Huntington Beach. The City of Huntington Beach maintains many of the collection lines that flow to the OCSD maintained trunk lines. OCSD operates both Plant 1 in Fountain Valley and Plant 2 in Huntington Beach, 27 lift stations throughout the City, and the large regional trunk lines running to the treatment facilities.

Plant 1 processes about 83 million gallons per day (mgd) and Plant 2 processes about 151 MGD. According to the Huntington Beach Sewer Master Plan, all projected buildout within the City of Huntington Beach is estimated to increase total flow by 1.95 MGD. Wastewater from the Specific Plan Area is treated at Plant 2. Staff at Plant 2 indicate that there are no current capacity issues and that the existing treatment facilities should, at minimum, meet area demand until 2050.

7.2.2.2. Wastewater Generation

The following wastewater demand factors were used to determine the total anticipated maximum flow produced by the Specific Plan Area. These factors will be required to size pipes within developments in conjunction with City standards. The Specific Plan Area could increase flows to 0.67 MGD during peak usage points, but average daily flow should fall between 0.3 MGD and 0.4 MGD. A peak hour demand of 0.67 MGD will not surpass the estimated 1.95 MGD planned for by the OCSD.

7.2.2.3. Wastewater Collection

The Specific Plan Area is served by a network of city lines that flow to a 54-inch trunk line in Walnut Avenue operated by the OCSD. The Public Works department has indicated that the 54-inch trunk was originally designed to support upstream development that was ultimately rerouted to an alternative line. Therefore, current demand on the line is significantly less than capacity and should support increases in flow from the Specific Plan Area without issue.

Existing collection lines within the Specific Plan Area vary in size from 8-inch and 10-inch in most streets to 15-inch in the abandoned portion of the Lake Street right-of-way between 1st Street and 2nd Street. City lines running southwest merge with the southeast flowing trunk line in

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